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STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

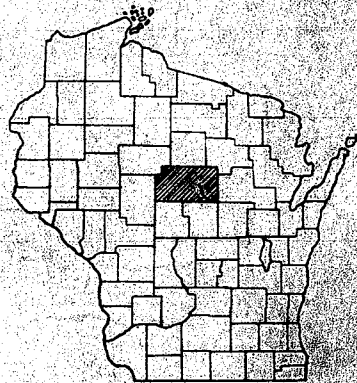
COUNTY AND HIGHWAY	AND SECTION	CLASS AND REFERENCE	FEDERAL	OFFICE	NUMBER
37.6	531-0		111	5N	10

PLAN AND PROFILE OF PROPOSED
S.T.H. 153 - CALLON
MARATHON COUNTY
PROJECT S 0531 (1)

APPROVED FOR

Marathon County

6/21/49
Date *Edwin Pillsbury*
Chairman

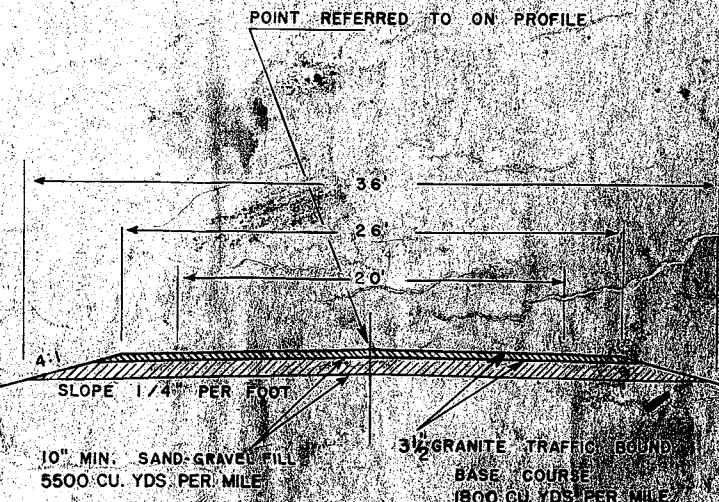
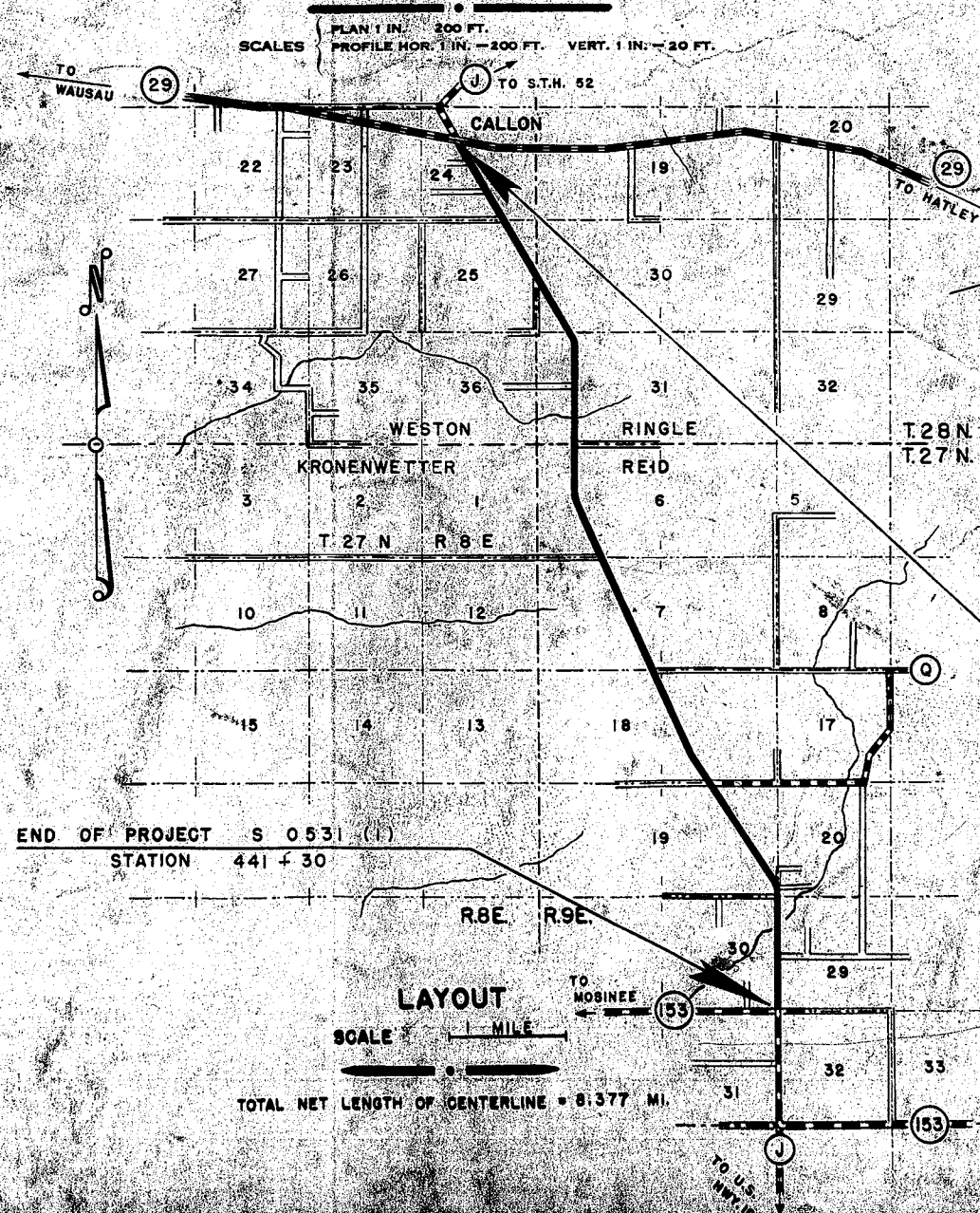


CONTRACT NO. 1

THIS PROJECT IS TO BE EXECUTED UNDER THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE HIGHWAY COMMISSION OF WISCONSIN EDITION OF 1941 APPROVED AS STANDARD SEPTEMBER 26, 1941, FEDERAL AID SPECIAL PROVISIONS APPROVED AS STANDARD NOVEMBER 8, 1948, AND SPECIAL PROVISIONS AS ATTACHED TO PROPOSALS.

ESTIMATE OF QUANTITIES

ITEM NO.	QUANTITY	UNIT	ITEM
2110 - 1	46,600	CU. YD.	SAND-GRAVEL FILL
SP	17,500	CU. YD.	DISINTEGRATED GRANITE BASE
2311 - 1	27,000	GAL.	BITUMINOUS MATERIAL - PRIME COAT
2312 - 3	123,000	GAL.	BIT. MAT. SURFACE COURSE & TACK COAT
2313 - 1	98,200	CU. YD.	WEARING SURFACE - CR. AGGR. ROAD MIX
2313 - 3	7,700	CU. YD.	AGGREGATE - CR. AGGR. BIT. SURFACE COURSE



TYPICAL SECTION

BEGINNING OF PROJECT S 0531 (1)
STATION 0+1+00
1740' EAST & 1030' NORTH OF THE WEST 1/4 CORNER OF SEC. 24

END OF PROJECT S 0531 (1)
STATION 44+30

LAYOUT

SCALE 1 MILE

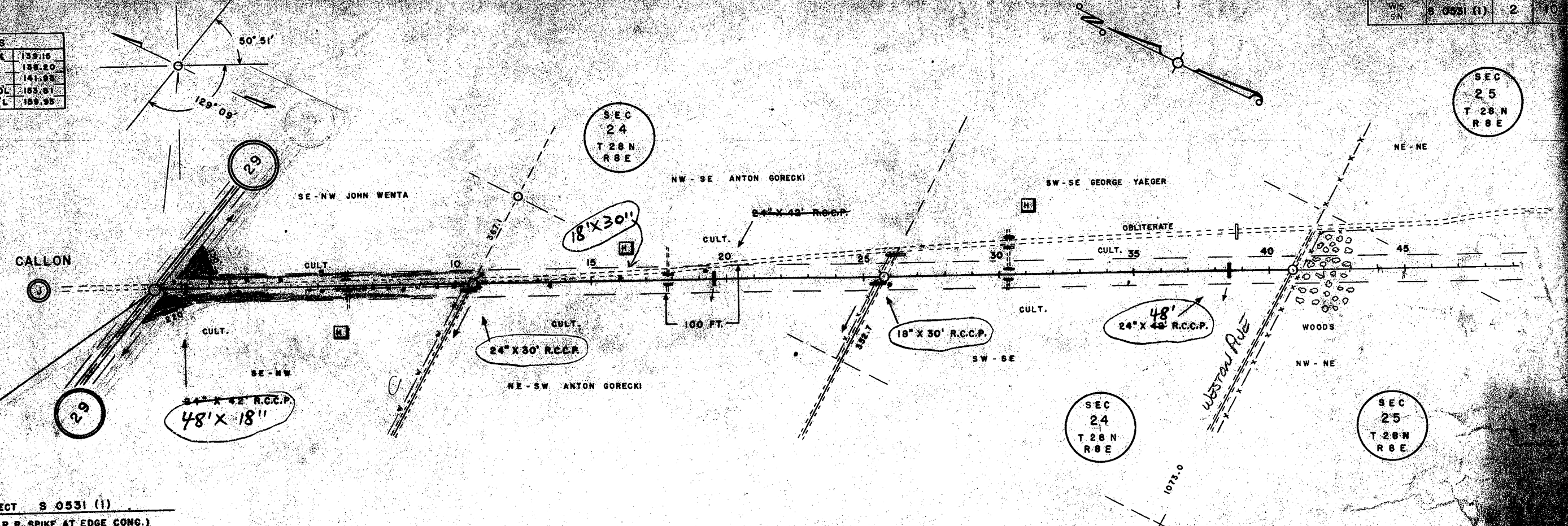
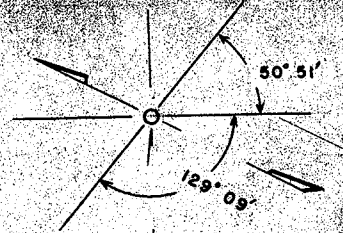
TOTAL NET LENGTH OF CENTERLINE = 8.377 MI.

CONVENTIONAL SIGNS

STATE LINE	--- ---	CULVERTS IN PLACE	--- ---
COUNTY LINE	--- ---	CULVERTS REQUIRED	--- ---
TOWNSHIP OR RANGE LINE	--- ---	DROP INLET	□
SECTION LINE	--- ---	POWER POLE	⊥
NEW RIGHT OF WAY LINE	--- ---	TELEPHONE OR TELEGRAPH POLE	⊥
PRESENT RIGHT OF WAY LINE	--- ---	RIGHT OF WAY MARKERS	▲
WIRE FENCE { WOVEN	--- ---	REFERENCE STAKE FOR HUBS ONLY	⊥
{ BARBED	--- ---	MARSH	~
LOT LINE	--- ---	HEDGE	~
CORPORATE OR CITY LIMITS	--- ---	TREES	⊙
PROPERTY LINE	--- ---	GROUND ELEVATION	--- ---
TRAVELED WAY OR P.E.	--- ---	GRADE ELEVATION	--- ---
RAILROADS	--- ---		
BASE OR SURVEY LINE	--- ---		

STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WIS.	FEDERAL WORKS AGENCY PUBLIC ROADS ADMINISTRATION
SURVEYOR: C.E.C. NOTE BOOK DIVISION COMPUTER COUNTY M.O. CHECKER DIVISION CHECKER: C.S.P. CORRECT	RECOMMENDED FOR APPROVAL: DATE
CORRECT: DATE: 6/21/49 W. J. Hazelton DIVISION ENGINEER	DISTRICT ENGINEER PUBLIC ROADS ADMINISTRATION FEDERAL WORKS AGENCY
RECOMMENDED FOR APPROVAL: DATE: 6/21/49	APPROVED: DATE
DESIGN ENGINEER: J. S. Pitt CONSTRUCTION ENGINEER	APPROVED: DATE: 6/21/49
APPROVED: E. C. Peterson STATE HIGHWAY ENGINEER	DESIGN ENGINEER PUBLIC ROADS ADMINISTRATION FEDERAL WORKS AGENCY

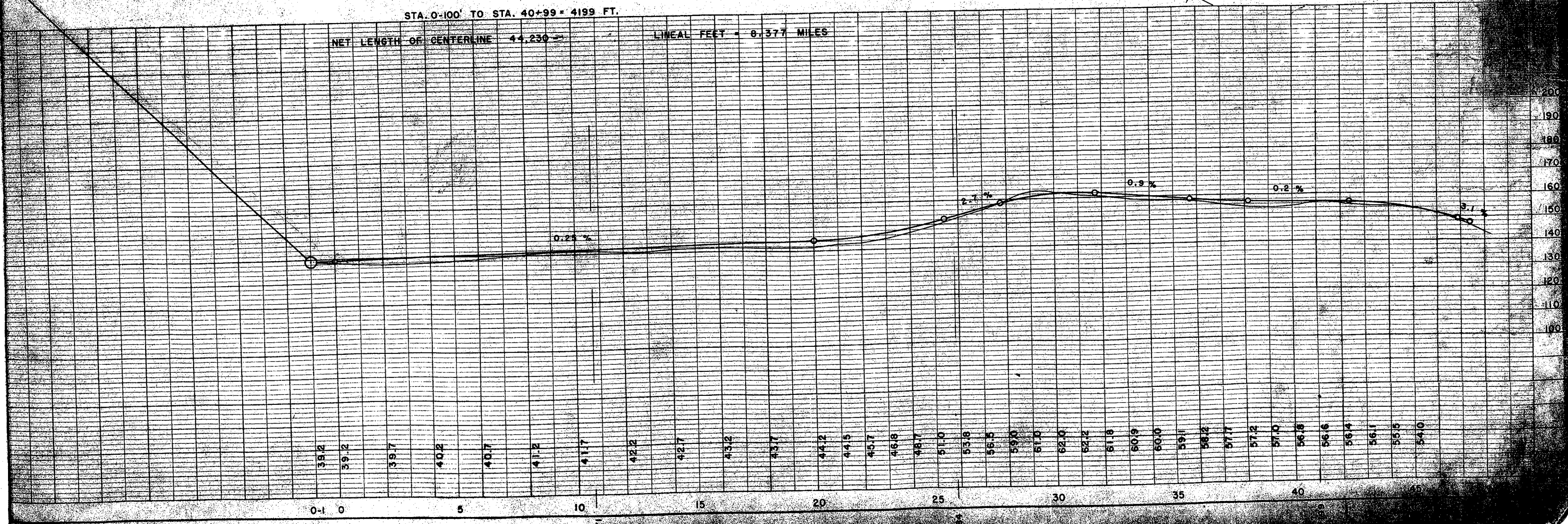
BENCH MARKS		
1	TOP CONC. HWY. 29	139.16
2	2-18 SPIKE IN	138.20
3	13-16 " " "	141.95
4	20-40 " PINE 150'L	153.91
5	26-50 " TW. OAK 120'L	159.95



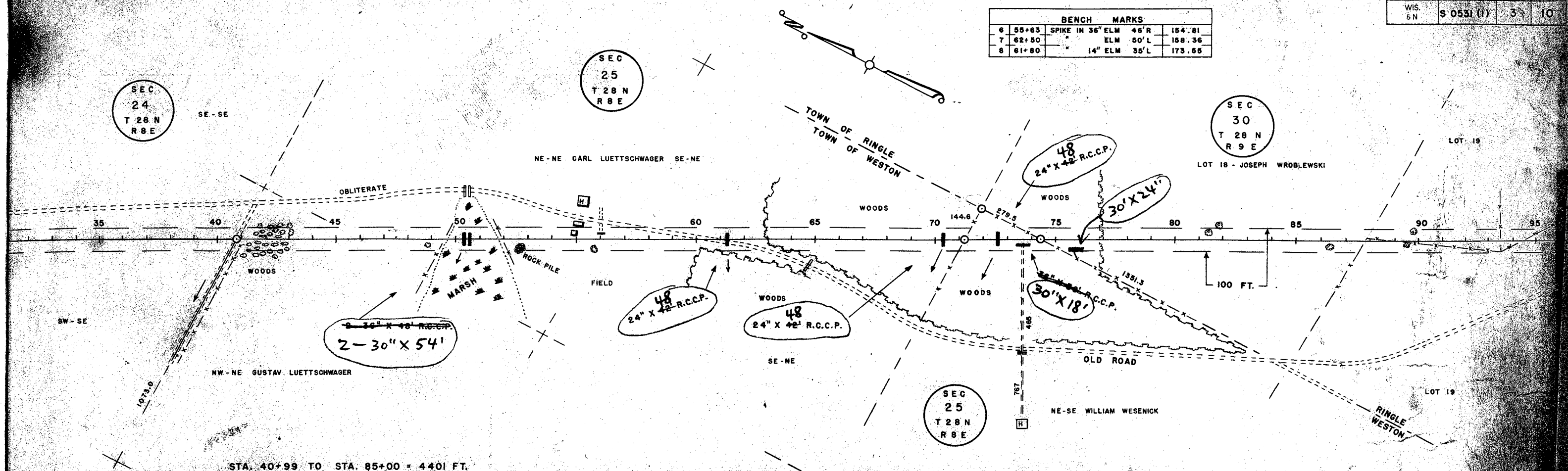
BEGINNING OF PROJECT 9 0531 (1)
STATION 0+100 (R.R. SPIKE AT EDGE CONC.)

STA. 0+100 TO STA. 40+99 = 4199 FT.

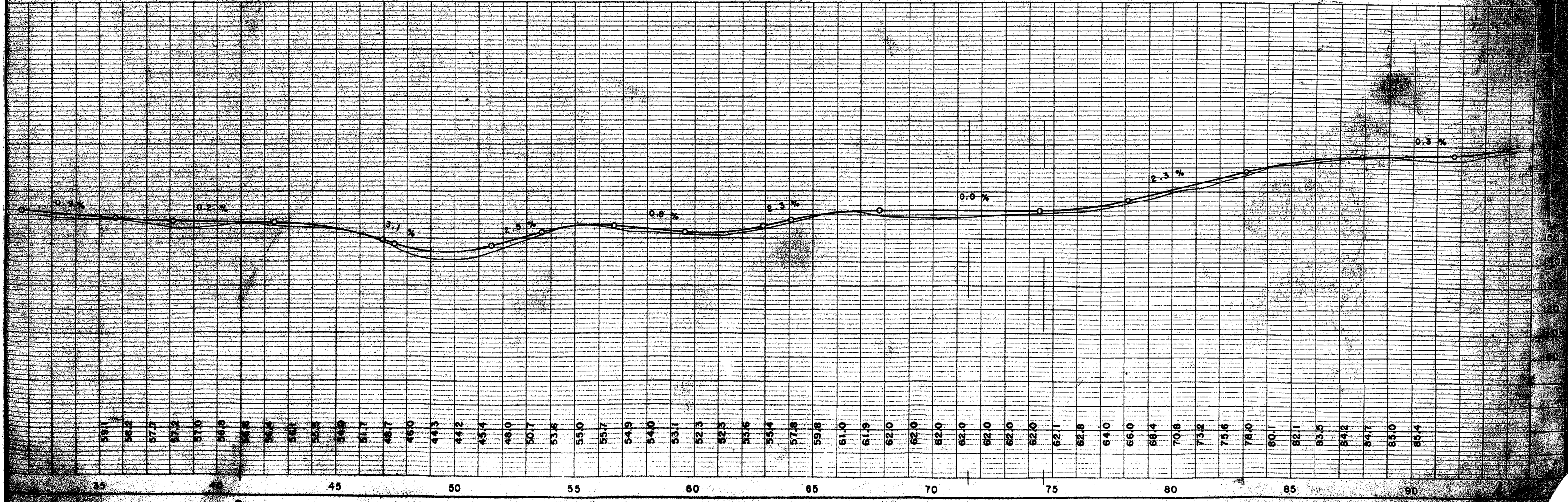
NET LENGTH OF CENTERLINE 44,230
LINEAL FEET = 8.377 MILES



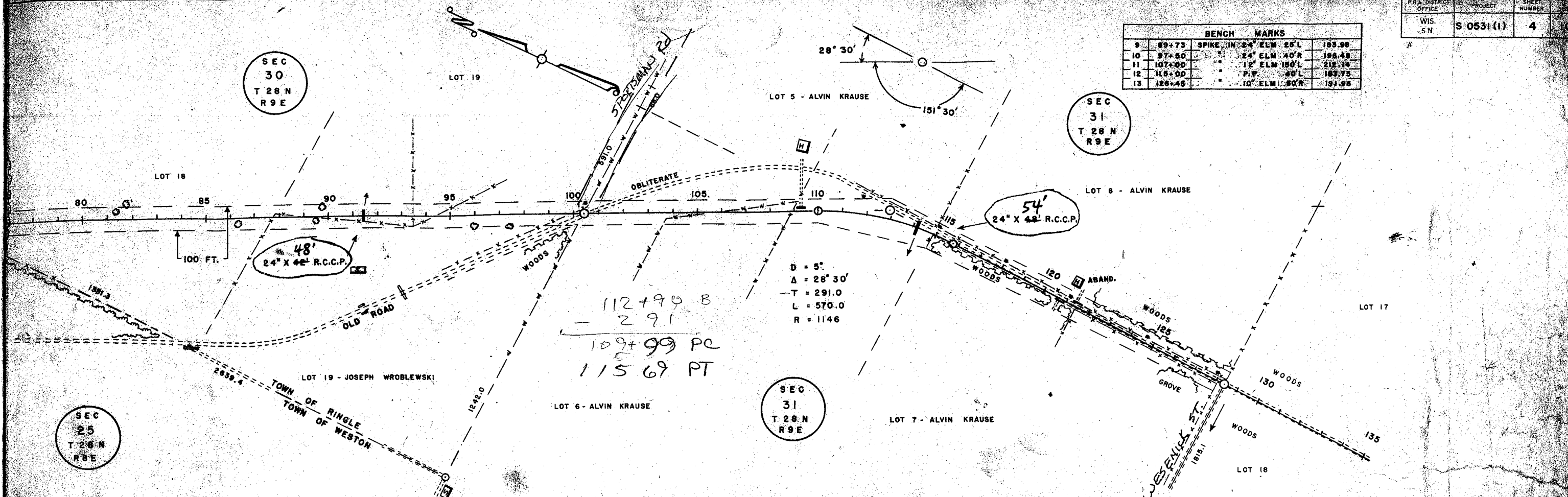
BENCH MARKS			
6	55+63	SPIKE IN 36" ELM 46' R	154.81
7	62+50	ELM 50' L	158.36
8	61+80	14" ELM 35' L	173.55



STA. 40+99 TO STA. 85+00 = 4401 FT.



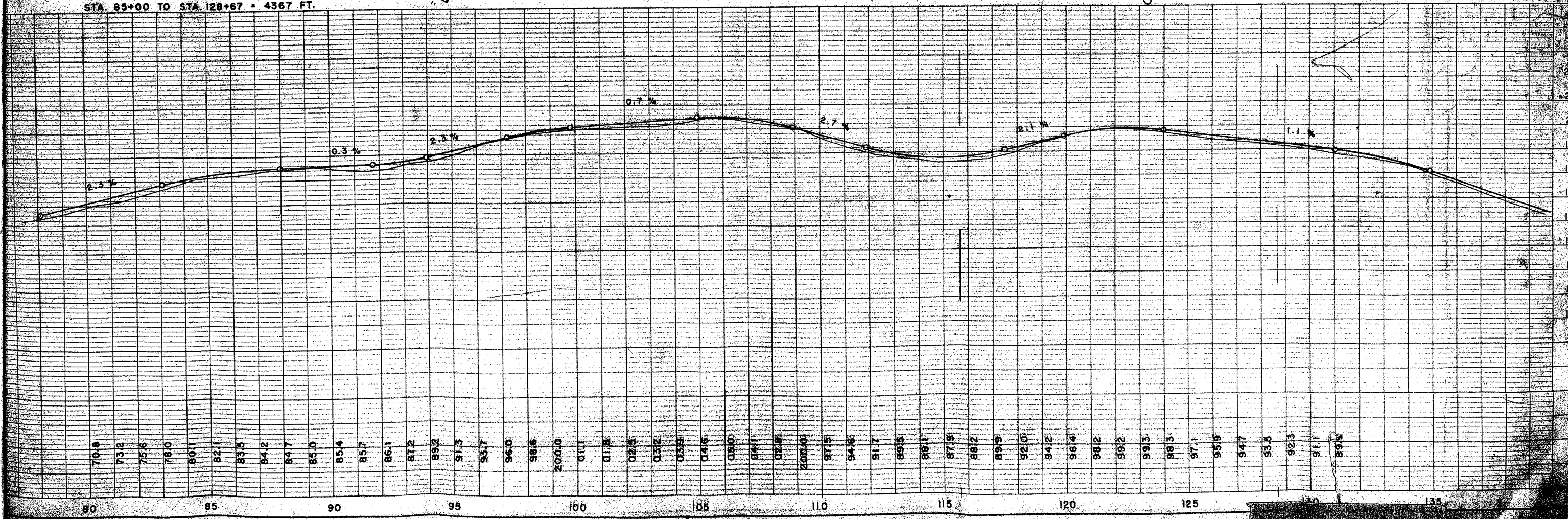
BENCH MARKS			
9	89+73	SPIKE IN 2" ELM 25'L	163.98
10	97+50	24" ELM 40'R	166.48
11	107+00	1" ELM 150'L	158.19
12	115+00	P.P. 50'L	169.75
13	126+45	10" ELM 50'R	171.95



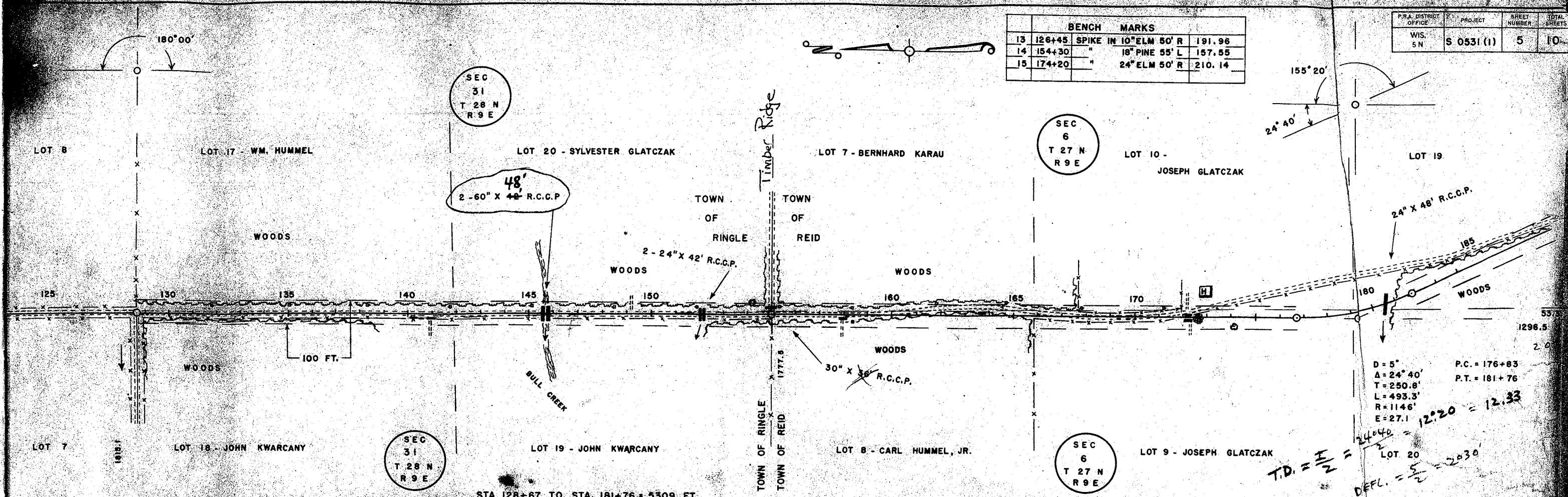
$D = 5'$
 $\Delta = 28^\circ 30'$
 $T = 291.0$
 $L = 570.0$
 $R = 1146$

$112+99 B$
 $- 291$
 $109+99 PC$
 $115 69 PT$

STA. 85+00 TO STA. 126+67 = 4367 FT.



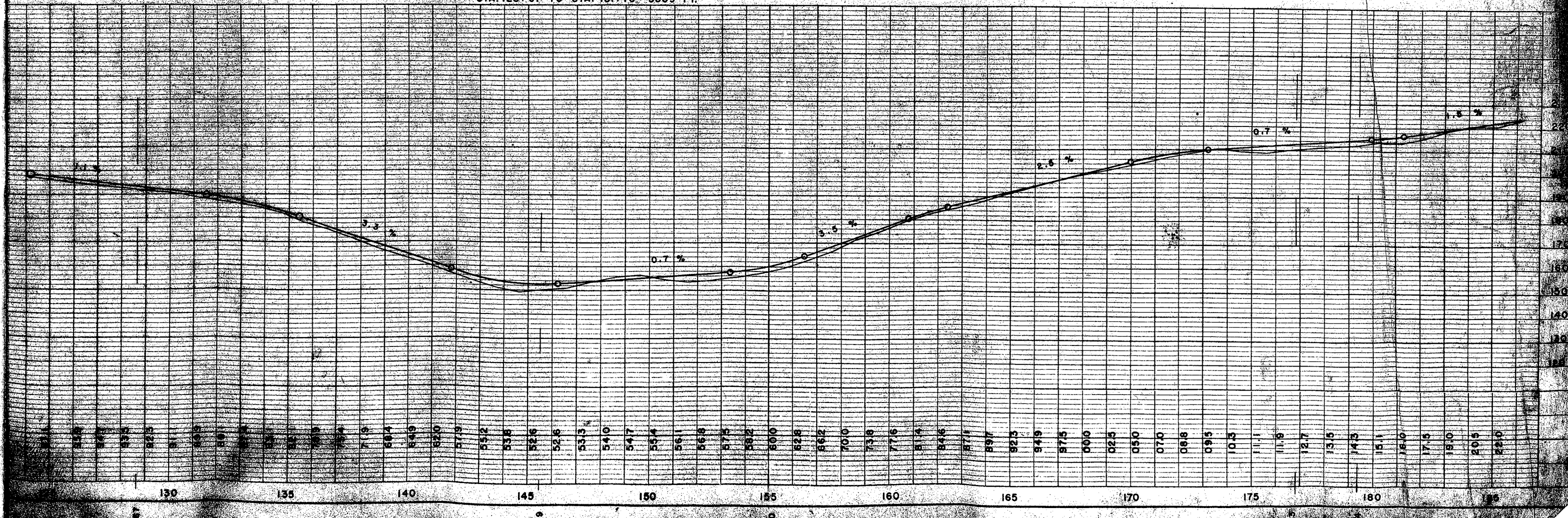
BENCH MARKS			
13	126+45	SPIKE IN 10" ELM 50' R	191.96
14	154+30	18" PINE 55' L	157.55
15	174+20	24" ELM 50' R	210.14



$D = 5'$
 $\Delta = 24^\circ 40'$
 $T = 250.8'$
 $L = 493.3'$
 $R = 1146'$
 $E = 27.1'$

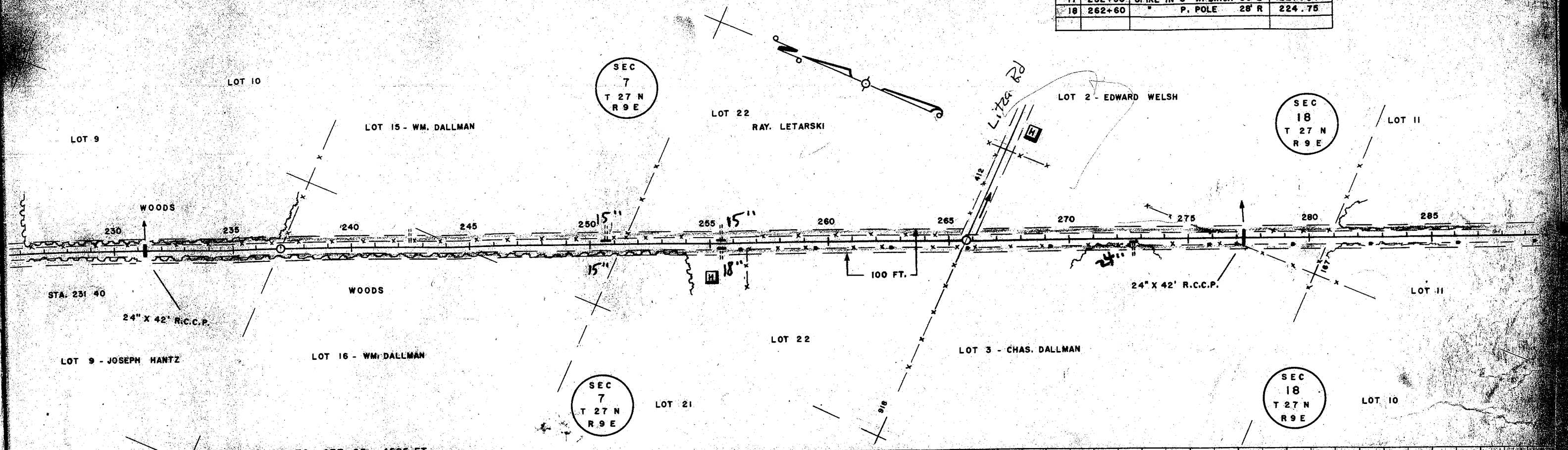
$P.C. = 176+83$
 $P.T. = 181+76$

$T.D. = \frac{L}{2} = \frac{493.3}{2} = 246.65$
 $DEFL. = \frac{L^2}{2R} = \frac{493.3^2}{2 \times 1146} = 203.0$

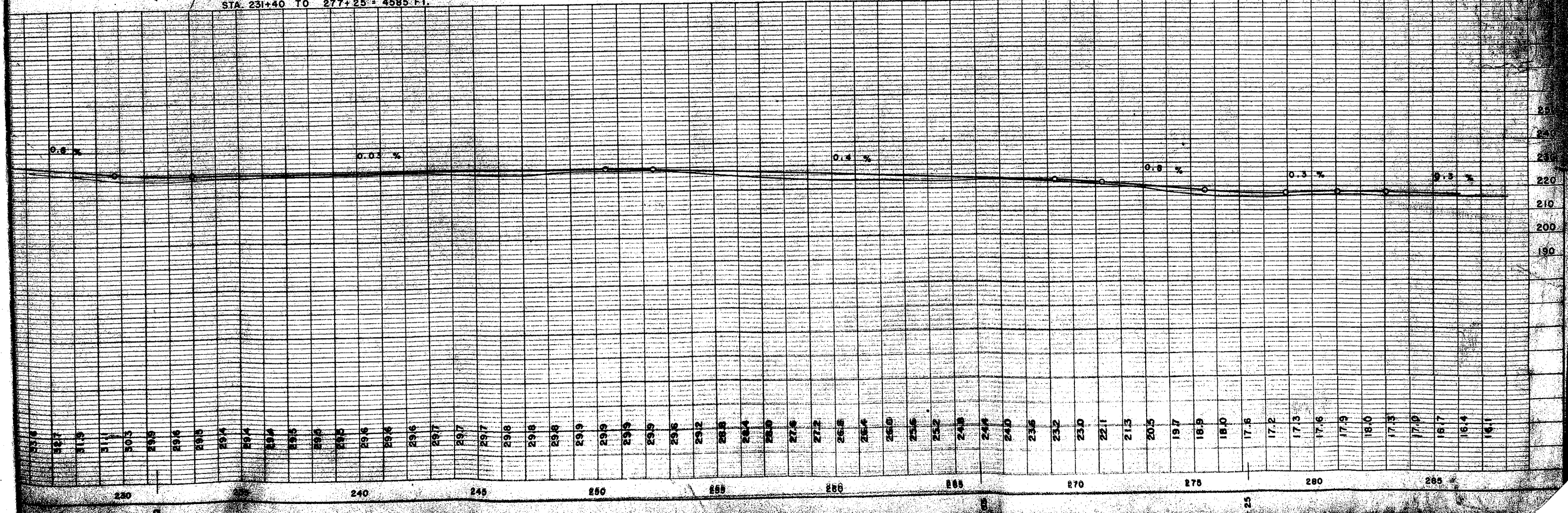


BENCH MARKS				
17	232+50	SPIKE IN 8" W. BIRCH	55' L	227.34
18	262+60	"	P. POLE 28' R	224.75

P.A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 5N	S 0531 (1)	7	10

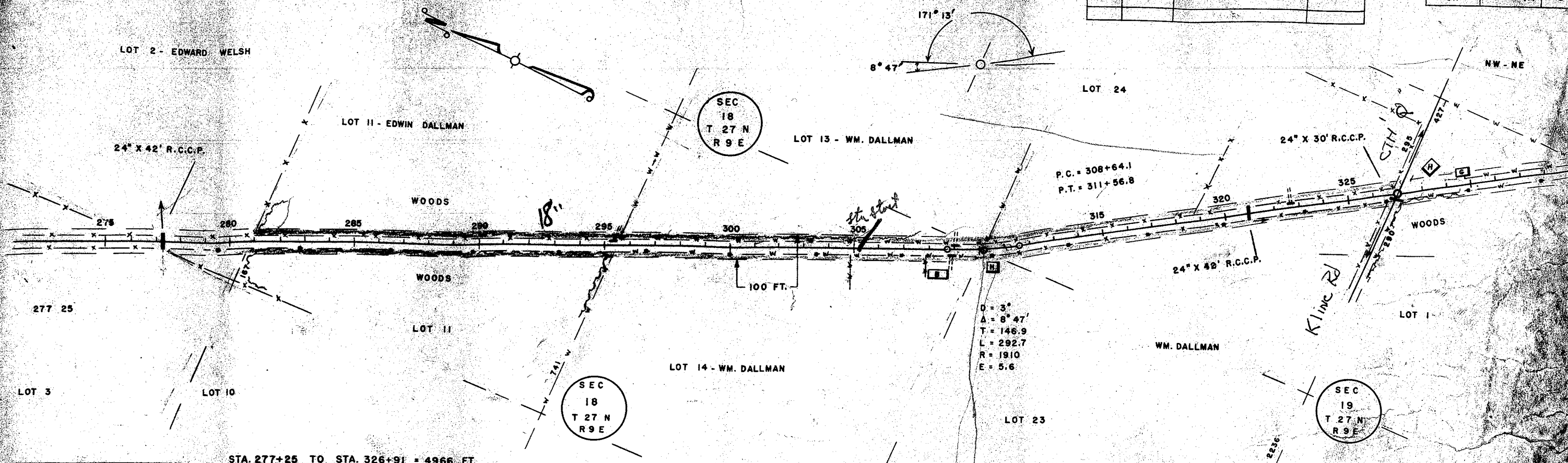


STA. 231+40 TO 277+25 = 4585 FT.

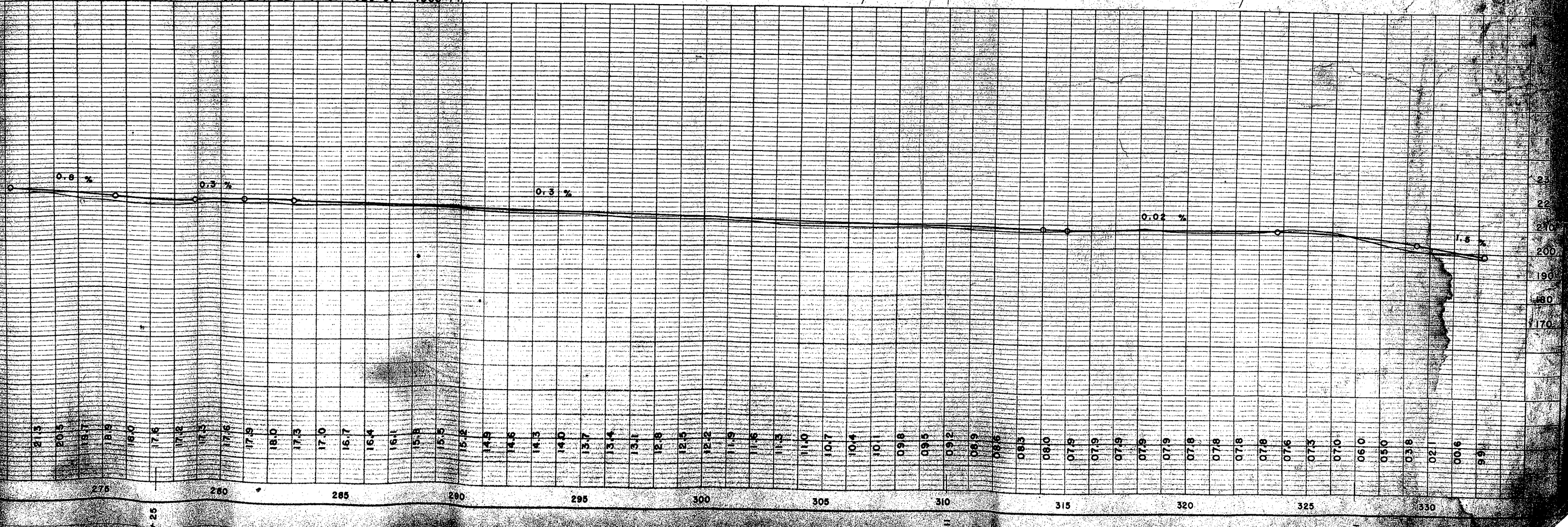


BENCH MARKS			
19	326+40	SPIKE IN P. POLE 28' R	207.37

P.R.A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS 5 N	S 0531 (1)	8	10



STA. 277+25 TO STA. 326+91 = 4966 FT.



Station	Elevation (ft)
275	215
276	205
277	197
278	189
279	180
280	176
281	173
282	170
283	167
284	164
285	161
286	158
287	155
288	152
289	149
290	146
291	143
292	140
293	137
294	134
295	131
296	128
297	125
298	122
299	119
300	116
301	113
302	110
303	107
304	104
305	101
306	99.8
307	99.5
308	99.2
309	98.9
310	98.6
311	98.3
312	98.0
313	97.9
314	97.9
315	97.9
316	97.9
317	97.8
318	97.8
319	97.8
320	97.8
321	97.6
322	97.5
323	97.0
324	96.0
325	95.0
326	93.8
327	92.1
328	90.6
329	89.1

BENCH MARKS			
21	384+30	SPIKE IN 12" ELM 30' L	154.37
22	428+25	18" PINE 45' R	138.95
23	440+15	P. POLE 45' L	133.58
24	440+50	TOP 2ND CHURCH STEP	134.03

SEC 29
T 27 N
R 9 E

SEC 30
T 27 N
R 9 E

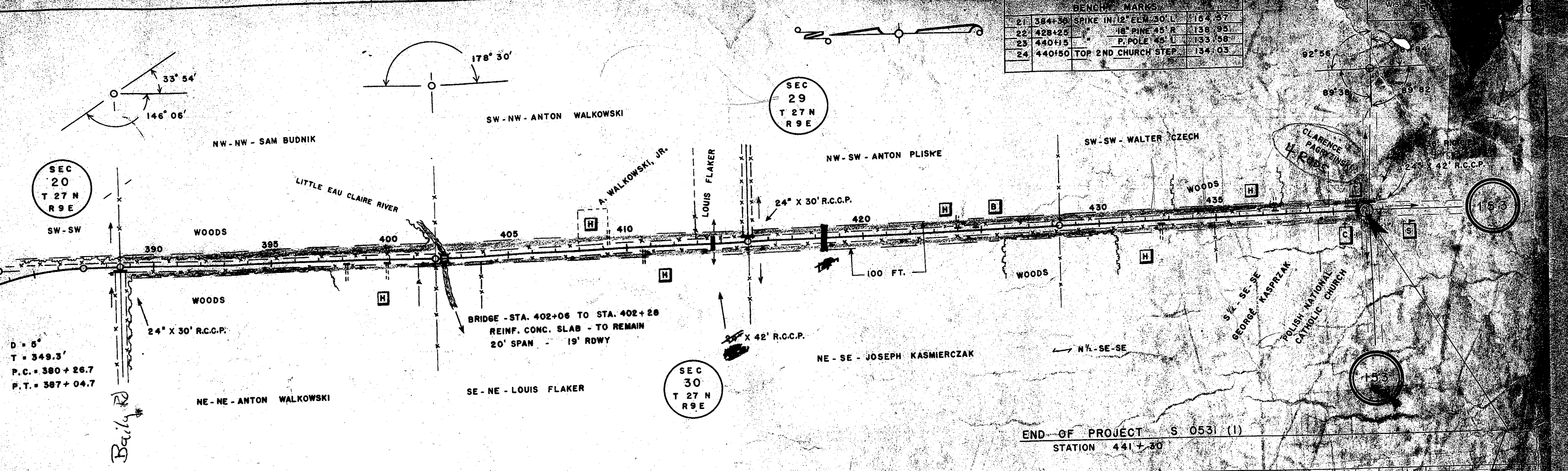
153

153

SEC 20
T 27 N
R 9 E

D = 5'
T = 349.3'
P.C. = 380 + 26.7
P.T. = 387 + 04.7

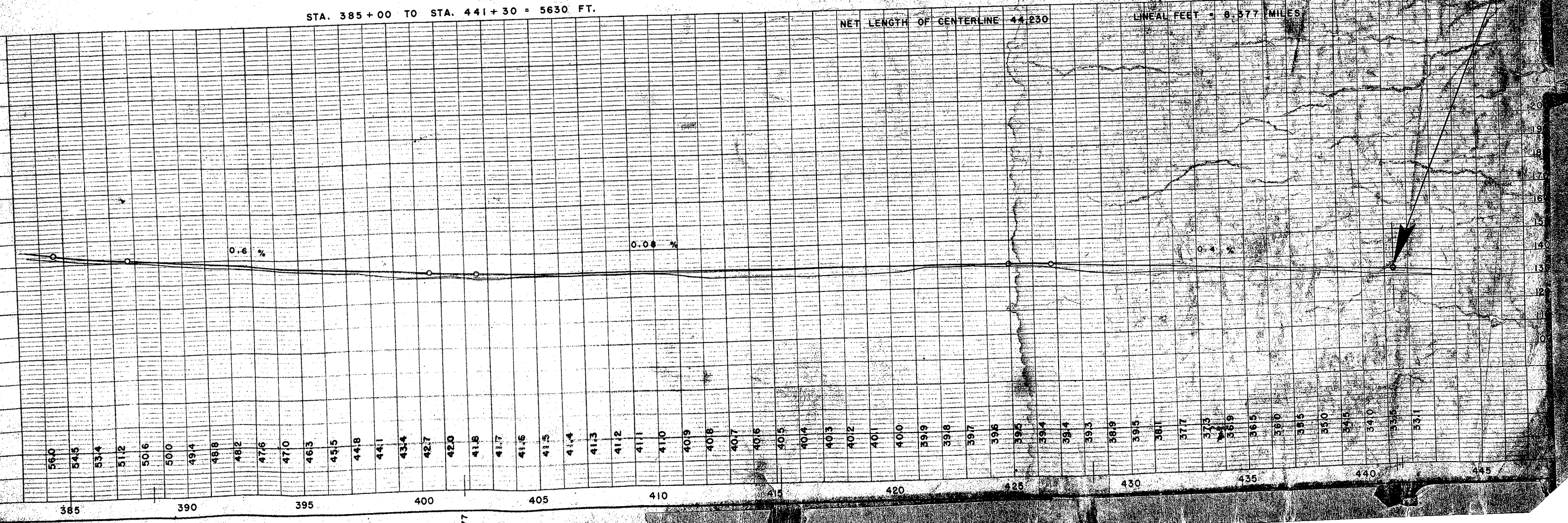
Baily Rd



STA. 385 + 00 TO STA. 441 + 30 = 5630 FT.

NET LENGTH OF CENTERLINE 44,230

LINEAL FEET = 8.577 MILES



0.6 %

0.08 %

0.4 %

560 545 534 512 506 500 494 488 482 476 470 463 455 448 441 434 427 420 415 417 416 415 414 413 412 411 410 409 408 407 406 405 404 403 402 401 400 399 398 397 396 395 394 394 393 389 395 387 377 373 369 365 360 355 350 345 340 335 331

385 390 395 400 405 410 415 420 425 430 435 440 445