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- SHEET NO. 6-9 DRAINAGE STRUCTURES
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STATE OF WISCONSIN  
STATE HIGHWAY COMMISSION OF WISCONSIN

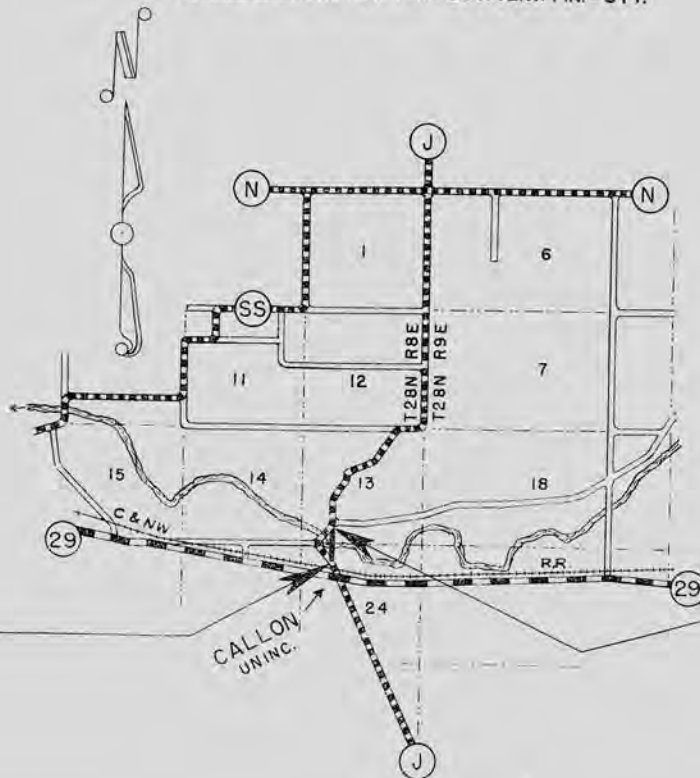
COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
37.6	531.0		11.3	WIS. 4	1	15

PLAN AND PROFILE OF PROPOSED

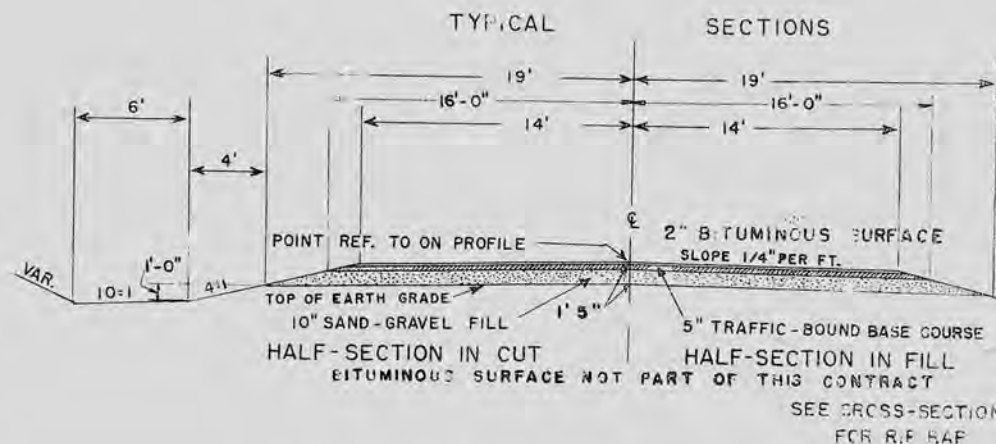
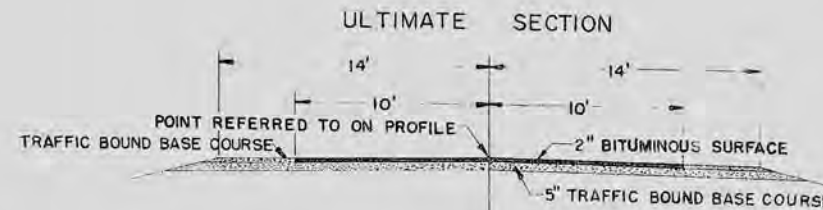
**S.T.H. "29" - S.T.H. "52"**  
**CALLON BRIDGE**  
MARATHON COUNTY

PROJECT S 0531 (3)

SCALES } PLAN 1 IN. = 100 FT.  
          } PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.  
          } CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



BEGINNING OF PROJECT S 0531 (3)  
STATION 7+00 - 1521.5' EAST & 977' SOUTH OF NW COR. SEC. 24



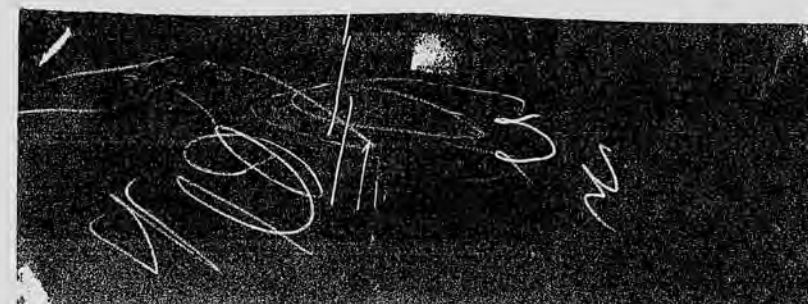
END OF PROJECT S 0531 (3)  
STATION 22+00  
1509.5' EAST & 513.8' N. OF SW COR. SEC. 13

APPLICABLE STANDARD DRAWINGS  
7-2.4.1 CLASS "B" STEEL PLATE BEAM GUARD  
7-4.1.1 CONSTRUCTION BARRICADE

CONVENTIONAL SIGNS

<p>STATE LINE.....</p> <p>COUNTY LINE.....</p> <p>TOWNSHIP OR RANGE LINE.....</p> <p>SECTION LINE.....</p> <p>NEW RIGHT OF WAY LINE.....</p> <p>PRES. RIGHT OF WAY LINE.....</p> <p>WIRE FENCE { WOVEN.....</p> <p>                  { BARBED.....</p> <p>L. &amp; T. LINE.....</p> <p>CORPORATE LIMITS.....</p> <p>PROJECT LINE.....</p> <p>TRAIL OR P.E. LINE.....</p> <p>FIELD SURVEY LINE.....</p> <p>BASE SURVEY LINE.....</p>	<p>CULVERTS IN PLACE.....</p> <p>CULVERTS REQUIRED.....</p> <p>DROP INLET.....</p> <p>POWER POLE.....</p> <p>TELEPHONE OR TELEGRAPH POLE.....</p> <p>RIGHT OF WAY MATCH.....</p> <p>REFERENCE STRIKE FOR P.O.B.S. ONLY.....</p> <p>MARSH.....</p> <p>HEDGE.....</p> <p>TREES.....</p> <p>GROUND ELEVATION.....</p> <p>SH. OR ELEVATION.....</p>
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LAYOUT  
SCALE 1 MILE  
TOTAL NET LENGTH OF CENTERLINE = 0.284 MI



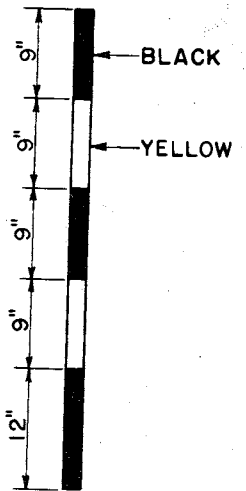
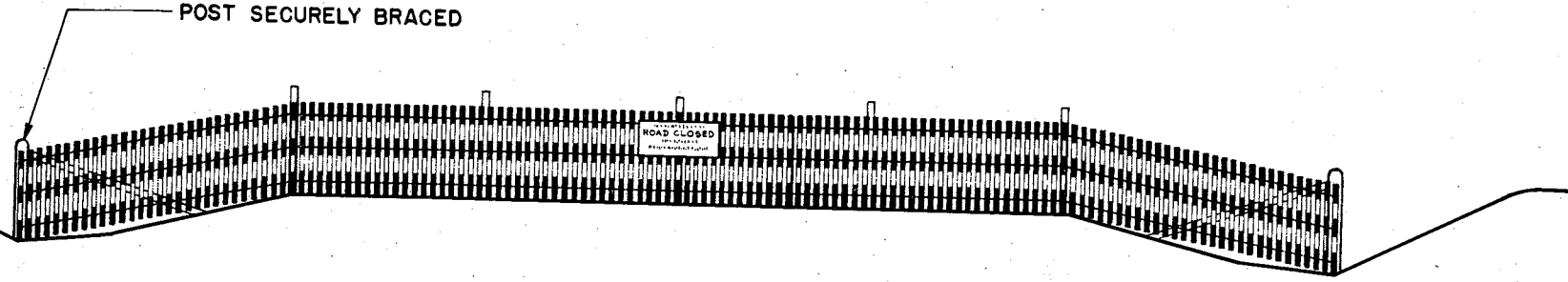
STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WISCONSIN	
SURVEYOR.....	C.E.C. NOTE BOY M-137
DISTRICT COMPUTER.....	C.E.C. O'NEILL
DISTRICT CHECKER.....	CORRECT
CORRECT.	
DATE.....	4/1/55 W. MacLean DISTRICT ENGINEER
RECORDED FOR APPROVAL	
DATE.....	2-22-55 J. J. Kelly ENGINEER OF DISTRICT
APPROVED:	
DATE.....	2/27/55 E. L. Rustigas DISTRICT ENGINEER
DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
APPROVED:	DATE.....
DISTRICT ENGINEER	

PROJECT S 0531 (3)

SECTION OF SNOW FENCE TO BE PAINTED ALTERNATELY BLACK AND YELLOW AS SHOWN BELOW.  
 LENGTH TO BE SUFFICIENT TO REACH FROM TOE OF THE BACKSLOPE TO TOE OF THE BACKSLOPE.  
 PROVIDE SUFFICIENT NUMBER OF INTERMEDIATE POSTS TO ADEQUATELY SUPPORT THE FENCE.

STANDARD "ROAD CLOSED" SIGN (W. H. C. - NO. R-15) TO BE ERECTED ON FENCE AT THE TOP ON  
 THE CENTERLINE OF ROAD. SIGN WILL BE FURNISHED BY COMMISSION.

POST SECURELY BRACED



DETAIL SHOWING METHOD OF STRIPING  
 FOR 4' SNOW FENCE.

STATE HIGHWAY COMMISSION OF WISCONSIN	
<b>CONSTRUCTION BARRICADE</b>	
RECOMMENDED FOR APPROVAL:	
<i>Frank Crase</i> DESIGN ENGINEER	
<i>W. J. Bluh</i> CONSTRUCTION ENGINEER	
APPROVED:-	
DRAWN <i>H.M.</i>	OCT. 1, 1945
CHECKED <i>WHG</i>	DATE <i>E. G. Follmer</i> STATE HIGHWAY ENGINEER

**GENERAL NOTES**

Details of construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and applicable Special Provisions. The Steel Plate Beam Guard shall consist of plate made of open hearth or electric furnace steel.

Plates shall be blanked to proper shape, fabricated and ready for assembly when received in the field. The plates shall be true to plan dimensions and of uniform section. Warped or deformed plates will be rejected. The edges of the plates shall be rolled or rounded so that they present no sharp edges. All connections and splices shall be formed with flat round headed bolts, or similar detail, so that no appreciable projection will be presented on the traffic side of the guard. The rail element shall be spliced by lapping in the direction of traffic or by butt joint with splice plate. The holes in the plate near the post shall be slotted to facilitate erection and to make provision for expansion and contraction. Plate ends in lap splices or plate ends and splice plate in butt splices shall make contact throughout the entire area of the splice.

**TESTS**

The elongation of a 2 inch specimen of the steel plate used in the rail element shall be not less than 12% when tested in tension.

The minimum tensile strength of the rail element shall, when tested in conjunction with splices and end connections, be 50,000 pounds. The rail element when loaded as a simple beam, freely supported at each end on 12'-6" centers, shall support a concentrated load of 2,000 pounds, applied at the center point, with a maximum deflection of 3 1/2 inches and shall support a concentrated load of 2,400 pounds, tested in like manner, with a maximum deflection of 5 1/2 inches.

**PAINTING**

**SHOP COAT**

Promptly following fabrication, the plates for steel rail element shall be thoroughly cleaned and shall be painted with a Red Lead Primer, or if an alternate of Red Lead Oxide Rust Inhibitive Primer or equivalent is used, the Engineer's prior approval shall be obtained. All parts, hardware and appurtenant fittings for the complete Beam Guard assembly shall likewise be painted when not furnished as galvanized.

**FIELD COAT**

Following erection, the steel rail elements and all parts, hardware and appurtenant fittings shall be painted in accordance with the Standard Specifications for Aluminum Paint, Section 4125.

Any damaged areas occurring to the shop coat during transportation or erection shall be cleaned and painted with an approved Rust Inhibitive Primer prior to any field coat painting.

Where the steel plate elements make contact with post mountings etc. all such areas which are impossible to paint after erection, shall be painted prior to erection.

All threaded portions of fittings and fasteners and cut ends of bolts shall be painted as specified or as directed by the Engineer.

**ALTERNATE DESIGNS**

Manufacturers may submit to the Engineer, for approval, designs for "Steel Plate Beam Guard" other than those shown on this drawing, providing that such alternate designs shall conform to the same physical tests and inspection requirements prescribed on this drawing for "Class B" Steel Plate Beam Guard."

**MEASUREMENT & PAYMENT**

The items of "Class B" Steel Plate Beam Guard" and "Class B" Steel Plate Beam (Median) Guard" shall be measured and paid for at the contract unit price per linear foot, measured in place by length in linear feet from end to end of Steel Plate or Steel Plates, which price shall be full compensation for furnishing all materials and performing all installation work to completion in accordance with the Plans and the Standard Specifications, Sections 2523 and 4124.

**BID ITEMS**

No. 2523-6 Steel Plate Beam Guard (Class "B").....Lin. Ft.  
No. 2523-7 Steel Plate Beam (Median) Guard (Class "B").....Lin. Ft.

**CLASS "B"  
STEEL PLATE BEAM GUARD &  
STEEL PLATE BEAM (MEDIAN) GUARD**

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

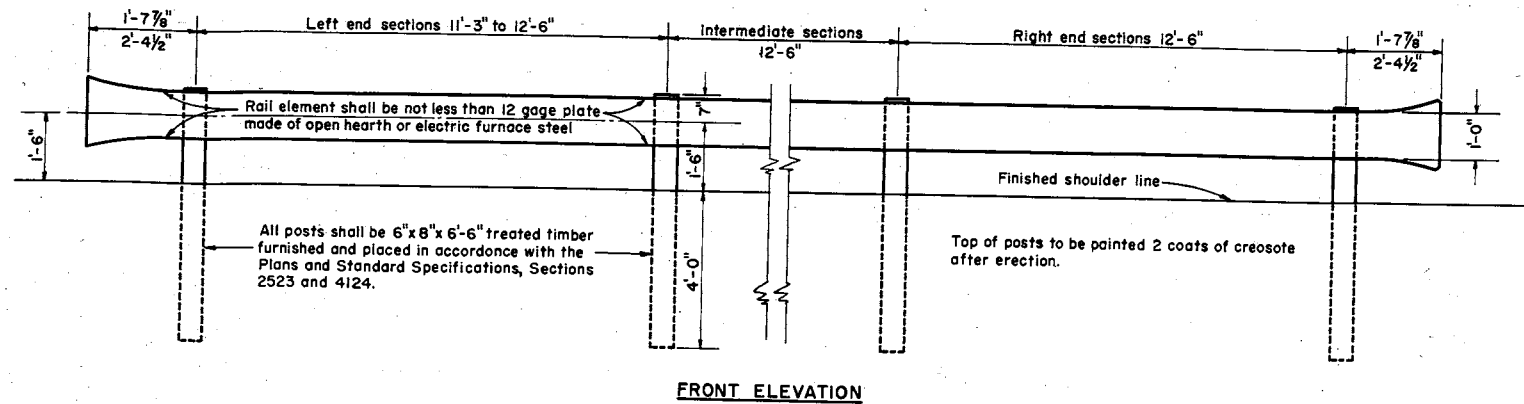
12-21-53  
DATE

J. J. Piff  
ENGINEER OF DESIGN

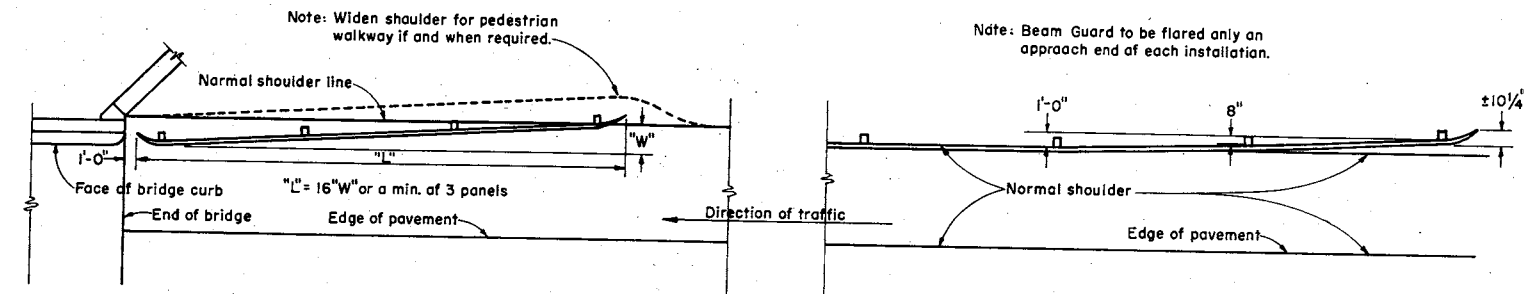
APPROVED:  
12/22/53  
DATE

Z. L. Rostker  
STATE HIGHWAY ENGINEER

PLATE NO. 7-2.4.1

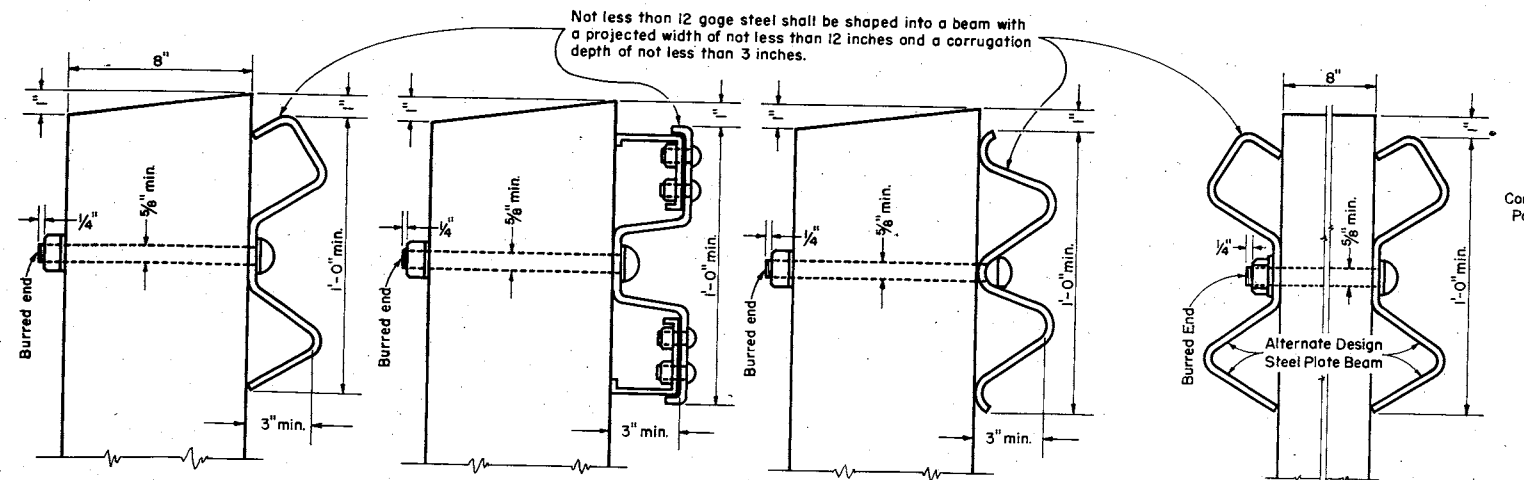


**FRONT ELEVATION**



**LOCATION DIAGRAM FOR BRIDGE APPROACHES**

**LOCATION DIAGRAM FOR INTERMEDIATE SECTIONS**



**SIDE ELEVATIONS**

**ALTERNATE DESIGNS-STEEL PLATE BEAM GUARD (CLASS "B")**

**SIDE ELEVATION**

**ALTERNATE DESIGN-STEEL PLATE BEAM (MEDIAN) GUARD (CLASS "B")**

**PLAN VIEW TERMINAL SECTIONS**



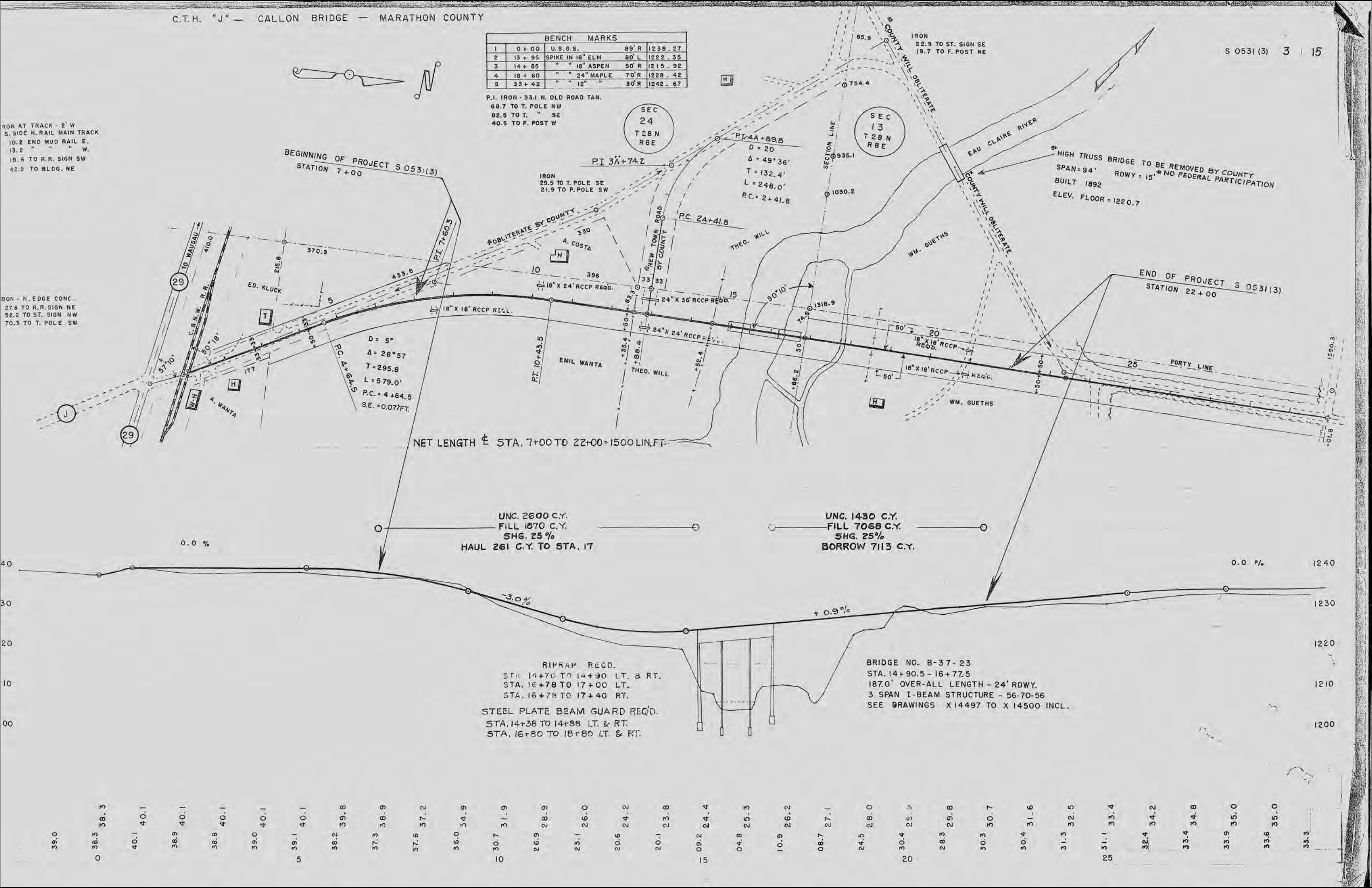
BENCH MARKS			
1	0+00	U.S.G.S.	89' R 1238.27
2	13+95	SPIKE IN 18" ELM	80' L 1222.35
3	14+85	" " 18" ASPEN	50' R 1215.92
4	18+60	" " 24" MAPLE	70' R 1228.42
5	33+42	" " 12"	30' R 1242.67

P.I. IRON - 33.1 N. OLD ROAD TAN.  
68.7 TO T. POLE NW  
82.5 TO T. " SE  
40.5 TO F. POST W



IRON AT TRACK - 2' W  
S. SIDE N. RAIL MAIN TRACK  
10.2 END MUD RAIL E.  
13.2 " " W.  
18.6 TO R.R. SIGN SW  
42.2 TO BLDG. NE

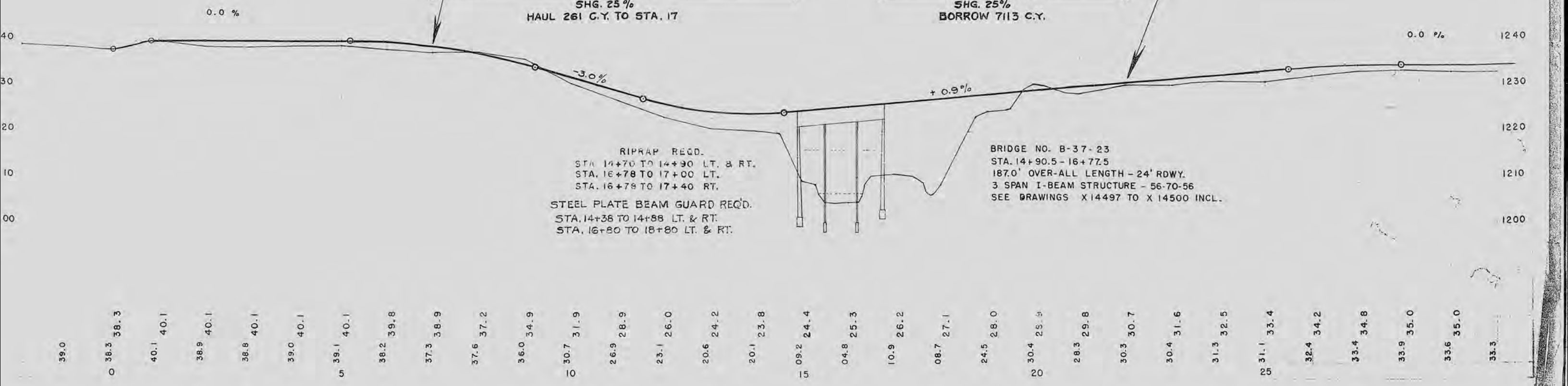
IRON - N. EDGE CONC.  
27.8 TO R.R. SIGN NE  
52.2 TO ST. SIGN NW  
70.3 TO T. POLE SW



NET LENGTH ± STA. 7+00 TO 22+00 = 1500 LIN. FT.

UNC. 2600 C.Y.  
FILL 1870 C.Y.  
SHG. 25%  
HAUL 261 C.Y. TO STA. 17

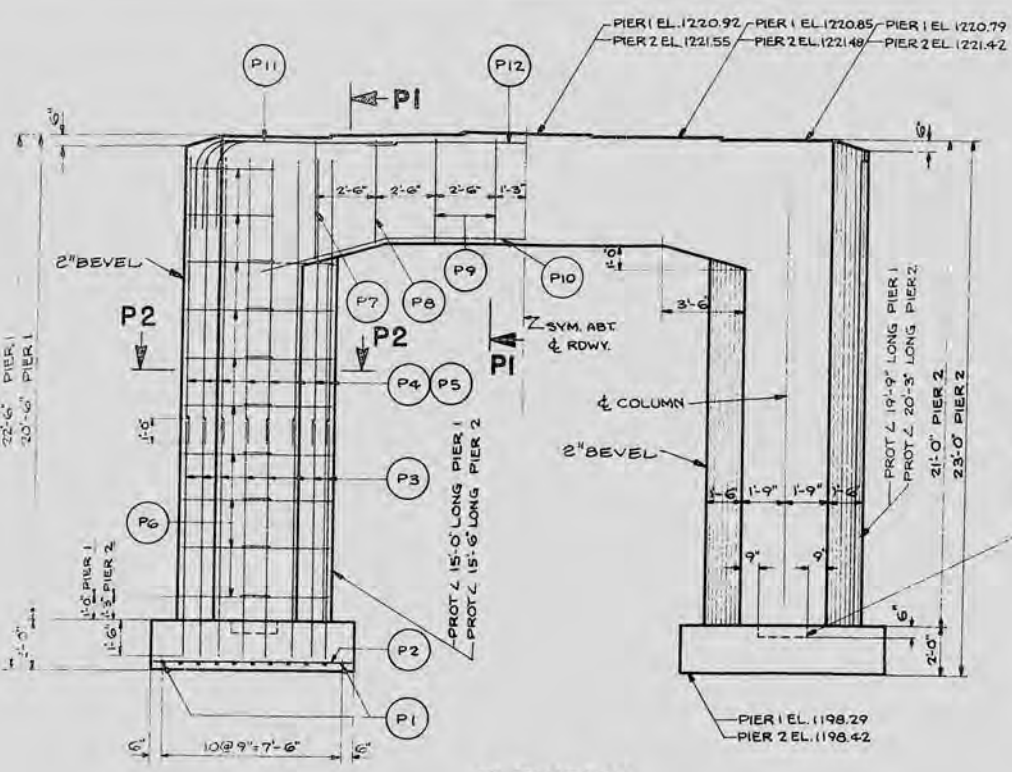
UNC. 1430 C.Y.  
FILL 7068 C.Y.  
SHG. 25%  
BORROW 7113 C.Y.



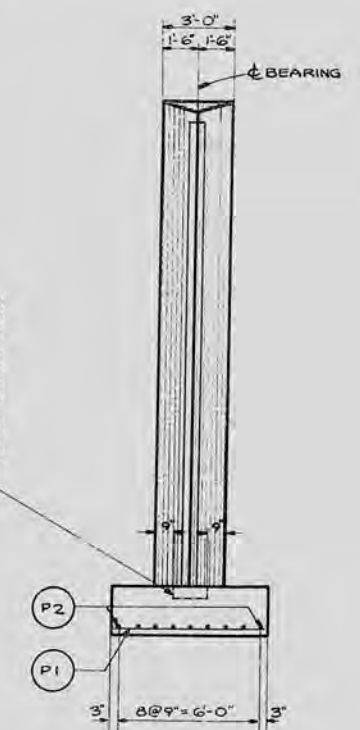
RIPPRAP REQD.  
STA. 14+70 TO 14+90 LT. & RT.  
STA. 16+78 TO 17+00 LT.  
STA. 16+78 TO 17+40 RT.  
STEEL PLATE BEAM GUARD REQD.  
STA. 14+38 TO 14+88 LT. & RT.  
STA. 16+80 TO 18+80 LT. & RT.

BRIDGE NO. B-37-23  
STA. 14+90.5 - 16+77.5  
187.0' OVER-ALL LENGTH - 24' RDWY.  
3 SPAN I-BEAM STRUCTURE - 56-70-56  
SEE DRAWINGS X 14497 TO X 14500 INCL.

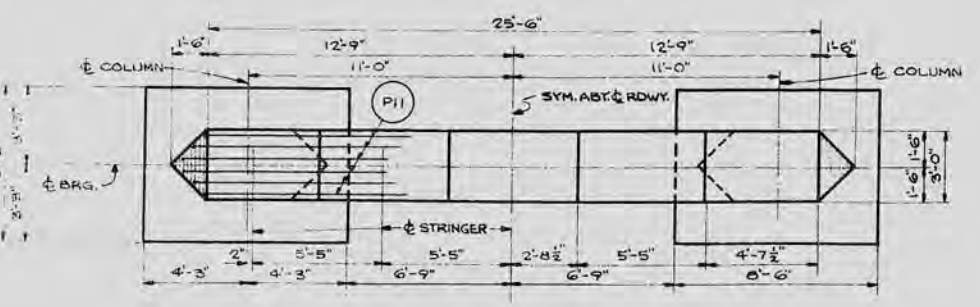
- 39.0
- 38.3
- 40.1
- 40.1
- 38.9
- 40.1
- 38.8
- 40.1
- 39.0
- 40.1
- 39.1
- 40.1
- 38.2
- 39.8
- 37.3
- 38.9
- 37.6
- 37.2
- 36.0
- 34.9
- 30.7
- 31.9
- 26.9
- 28.9
- 23.1
- 26.0
- 20.6
- 24.2
- 20.1
- 23.8
- 09.2
- 24.4
- 04.8
- 25.3
- 10.9
- 26.2
- 08.7
- 27.1
- 24.5
- 28.0
- 30.4
- 29.9
- 28.3
- 29.8
- 30.3
- 30.7
- 30.4
- 31.6
- 31.3
- 32.5
- 31.1
- 33.4
- 32.4
- 34.2
- 33.4
- 34.8
- 33.9
- 35.0
- 33.6
- 35.0
- 33.3



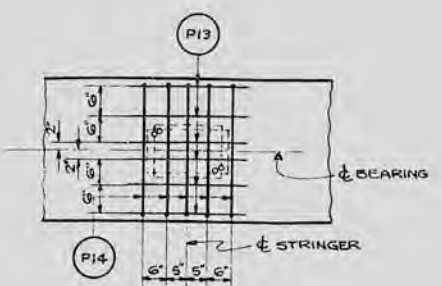
**ELEVATION**  
LOOKING NORTH



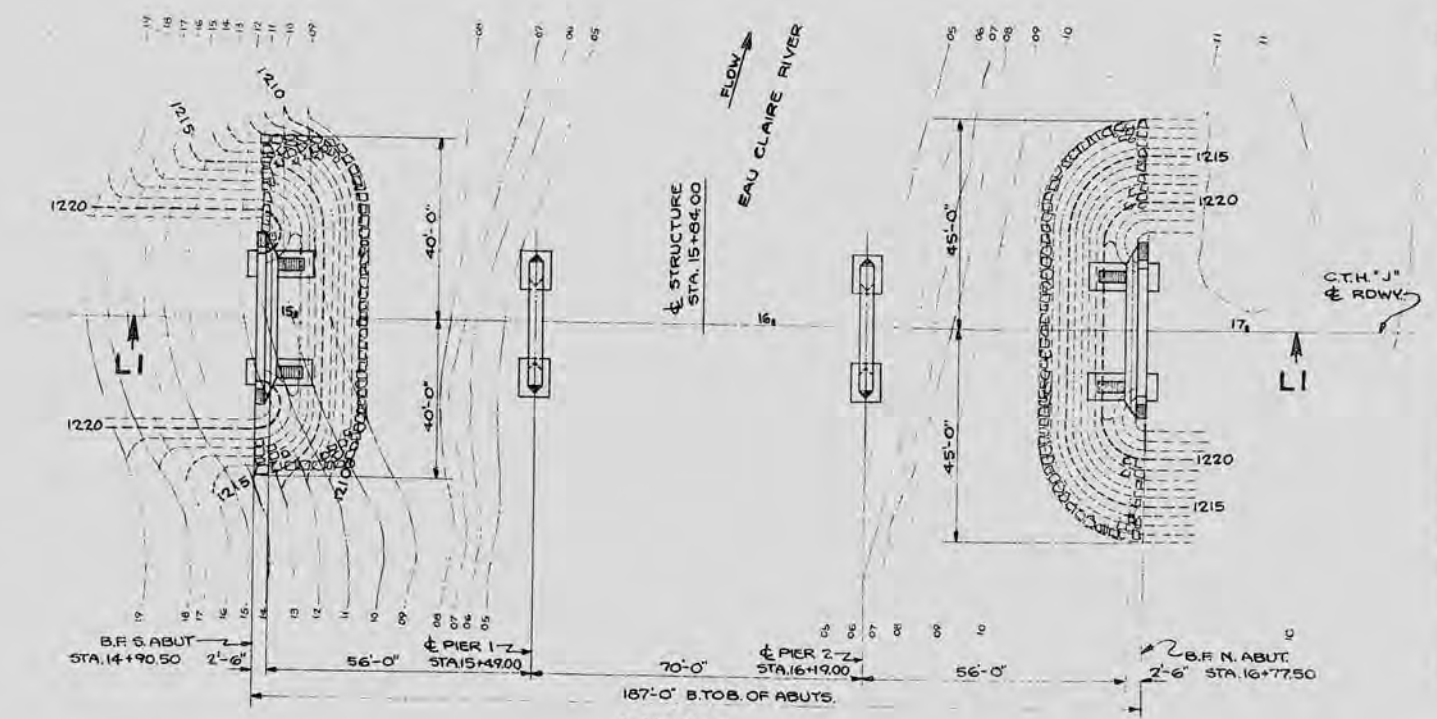
**END VIEW**



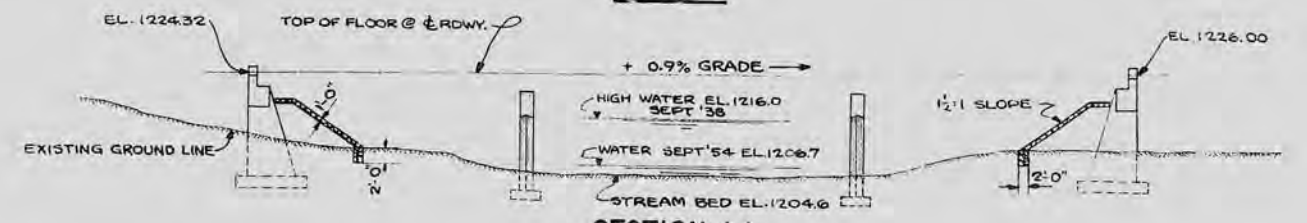
**PLAN**



**GRID DETAILS**  
OMIT 'P13' BARS UNDER EXTERIOR STRINGERS



**LAYOUT**



**SECTION LI**

**GENERAL NOTES**

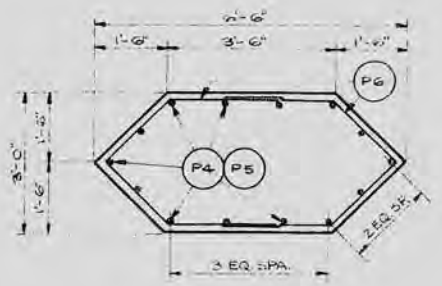
DRAWINGS SHALL NOT BE SCALED  
 ALL CONCRETE MASONRY SHALL BE GRADE 'AA'.  
 BEVEL EXPOSED EDGES OF CONCRETE 1" UNLESS OTHERWISE SPECIFIED.  
 ALL BAR STEEL REINFORCEMENT SHALL BE IMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.  
 INTERFERING BAR STEEL IN FLOOR SLAB SHALL BE CUT OFF AT FLOOR DRAINS.  
 ALL SHOP OR FIELD RIVETS SHALL BE 3/4" UNLESS OTHERWISE SPECIFIED.  
 ALL SUBSTRUCTURE UNITS SHALL BE KEYPED 12" INTO ROCK.  
 THE BERM IN FRONT OF THE ABUTMENTS SHALL BE RIPRAPPED AS SHOWN ABOVE. THE FILL AROUND THE ENDS OF THE WINGS SHALL BE RIPRAPPED TO A MINIMUM THICKNESS OF 6" AS DIRECTED BY THE ENGINEER IN THE FIELD.

1" x 3/4" x 1'-0" ANCHORS CONT. WELD TO L. PLACE ONE SET OF ANCHORS 7'-0" FROM EACH END AND ADDITIONAL SETS EQUALLY SPACED BETWEEN END SETS.

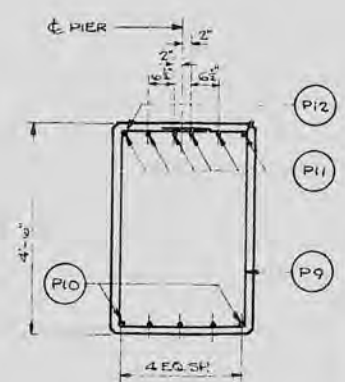
3/8" HOLES FOR BOLTING 25 TO FORMS. PUNCH ONE SET OF HOLES 6" FROM EACH END AND ADDITIONAL SETS MIDWAY BETWEEN ADJACENT ANCHORS.

ONE L 19'-9" LONG 7 SETS ANCHORS } PIER 1  
 ONE L 15'-0" LONG 5 SETS ANCHORS }  
 ONE L 20'-3" LONG 3 SETS ANCHORS } PIER 2  
 ONE L 15'-6" LONG 6 SETS ANCHORS }

**PROTECTION ANGLE DETAILS**



**SECTION P2**



**SECTION P1**

**TOTAL ESTIMATED QUANTITIES**

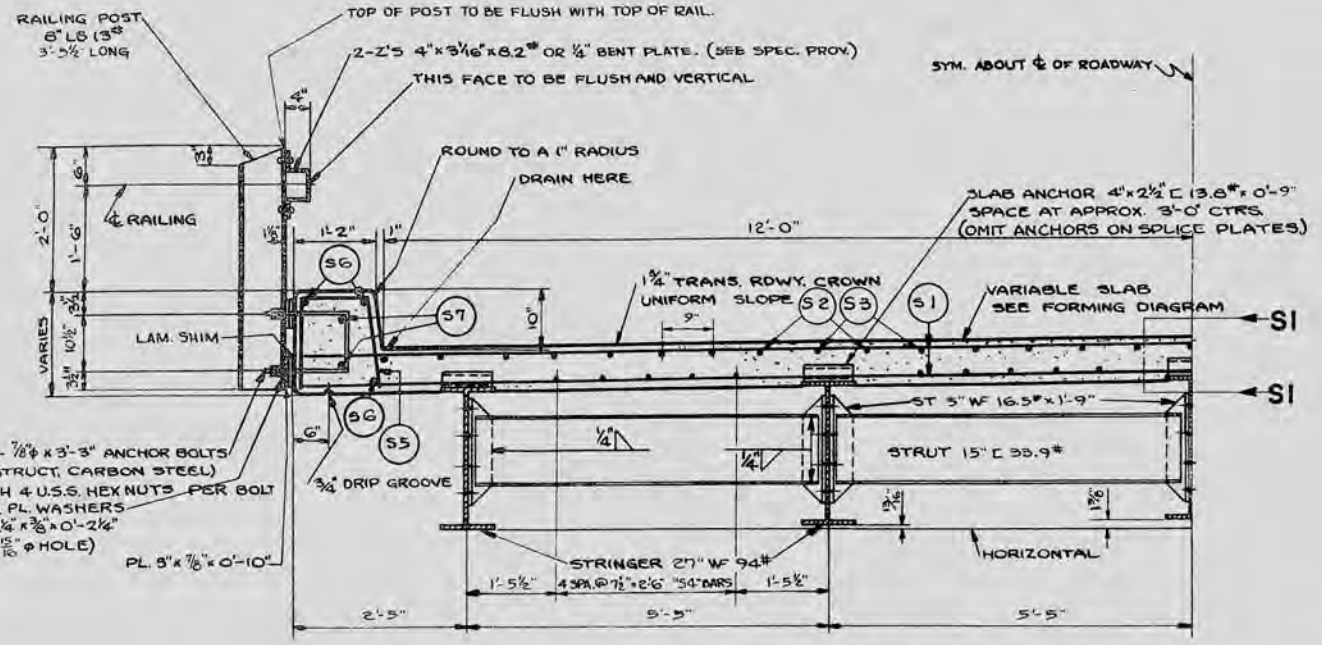
BID ITEMS	UNIT	SUPER	S. ABUT.	PIER 1	PIER 2	N. ABUT.	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.		115	55	55	90	315
CONCRETE MASONRY	C.Y.	126.8	57.7	39.9	40.4	57.5	322.3
BAR STEEL REINFORCEMENT	LB.	25,410	3010	2570	2600	3010	36,600
STRUCTURAL CARBON STEEL	LB.	126,900		550	570		128,020
SHEET LEAD	LB.	283					283
SHEET ZINC	LB.	94					94
FLOOR DRAINS	EA.	6					6
RIPRAP	C.Y.		80			95	175
NON-BID ITEMS							
EXPANSION JOINT FILLER	SIZE	1"					1"

**LIST OF DRAWINGS**

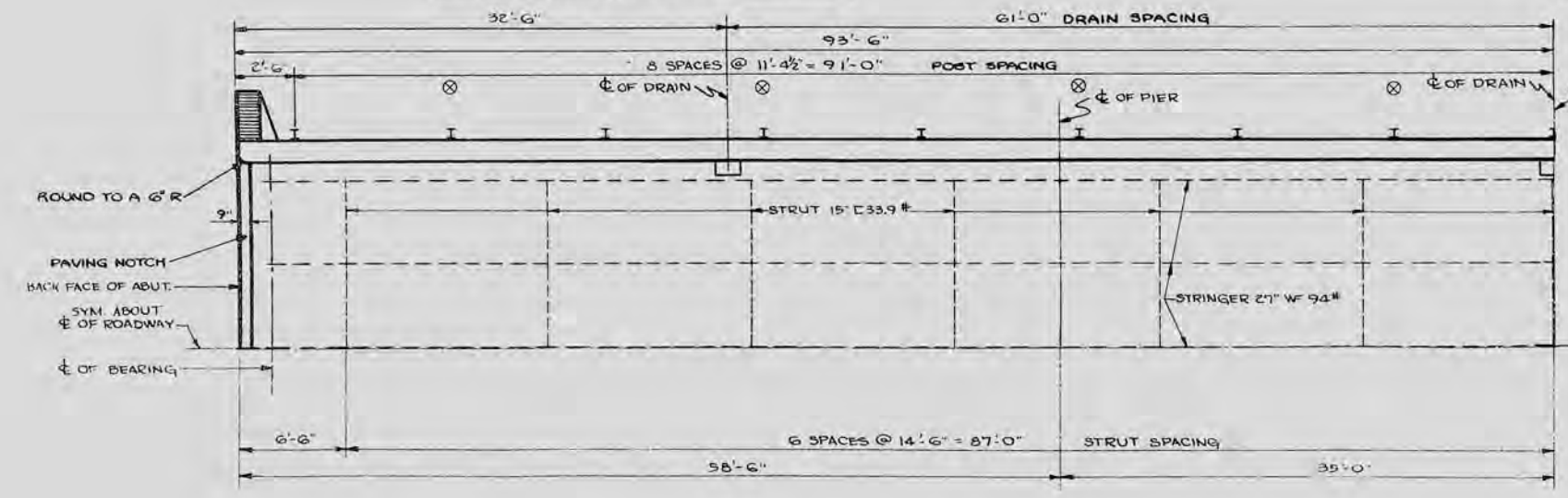
- 1. LAYOUT & PIERS X14497
- 2. SUPERSTRUCTURE X14498
- 3. SUPERSTRUCTURE X14499
- 4. ABUTMENTS & BILL OF BARS X14500

THE DESIGN OF THIS STRUCTURE IS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES A.A.S.H.O. EDITION OF 1953.

STATE OF WISCONSIN  
**LAYOUT & PIERS**  
 MARATHON WESTON 15+84.00  
 24 28N 8E  
 1217.54 WM 10 L.W.F. 1951 10-0 M15  
 G. W. K...  
 E. L. P...  
 STRUCTURE B-37-23 SHEET 1 OF 4

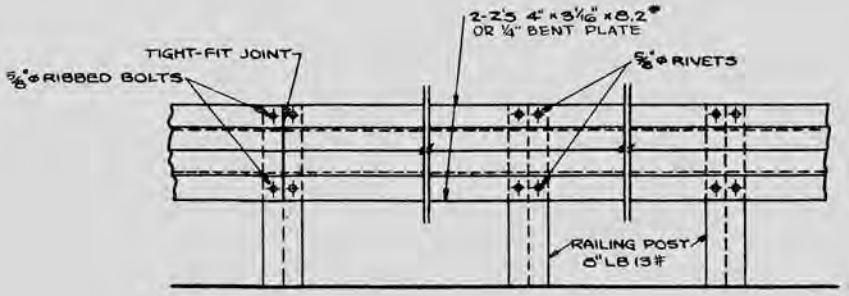


**HALF CROSS SECTION OF RDWY.**

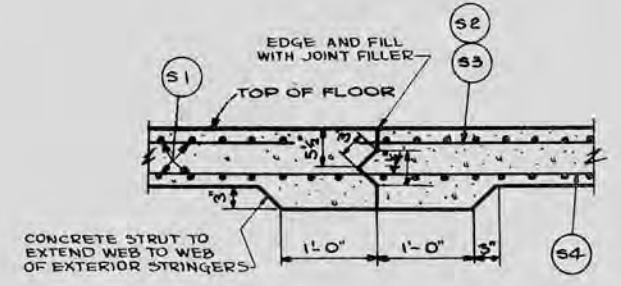


**QUARTER PLAN**

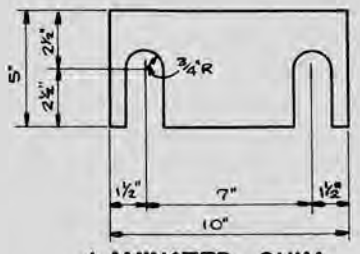
CONSTRUCTION JOINTS NOT SHOWN - SEE POURING DIAGRAM



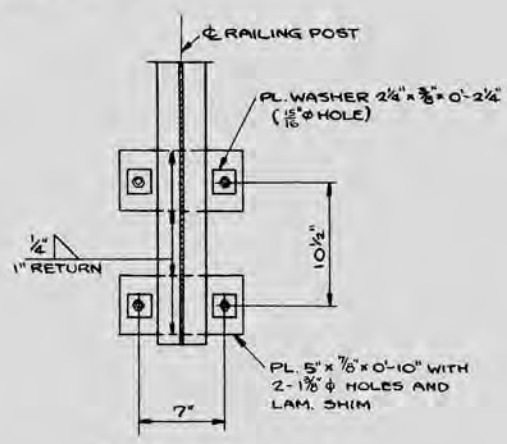
**AT SPLICES** **TYPICAL**  
**RAILING DETAILS**



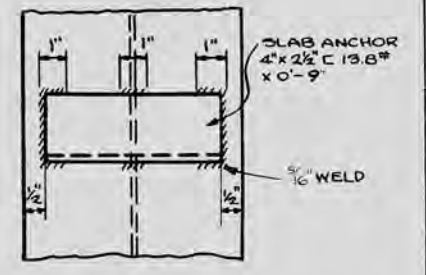
**CONST. JOINT**



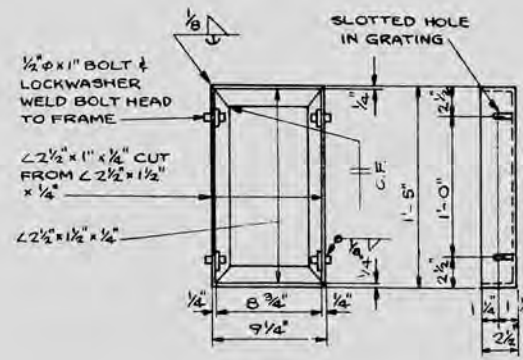
**LAMINATED SHIM**



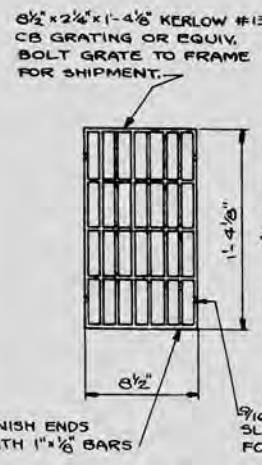
**POST DETAILS**



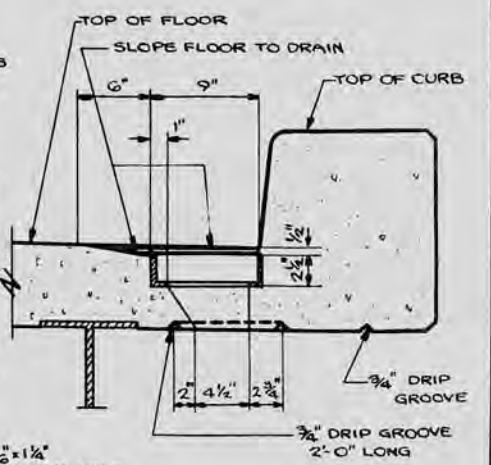
**SLAB ANCHOR DETAILS**



**FRAME**

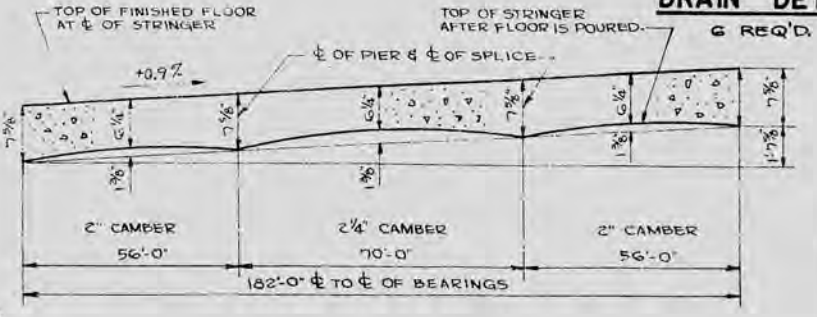


**GRATE**



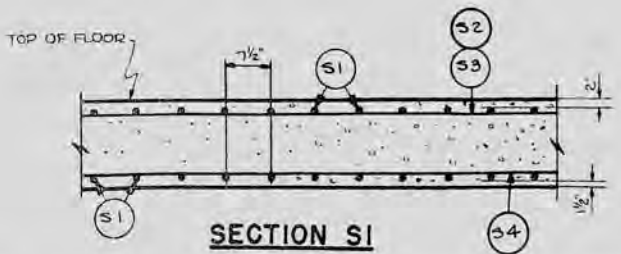
**SECTION THRU DRAIN**

**DRAIN DETAILS**

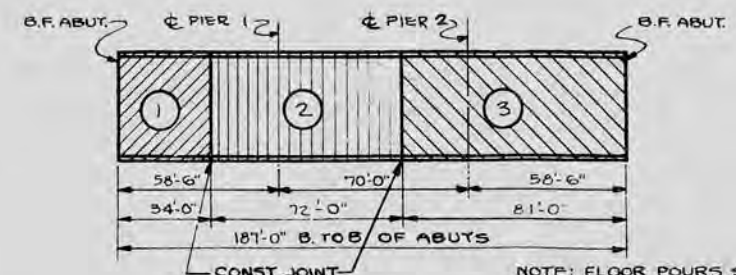


**FORMING DIAGRAM**

NOTE: THE CAMBER IN THE STRINGERS IS BEFORE ERECTION AND SHALL CONFORM TO THE ARC OF A CIRCLE.



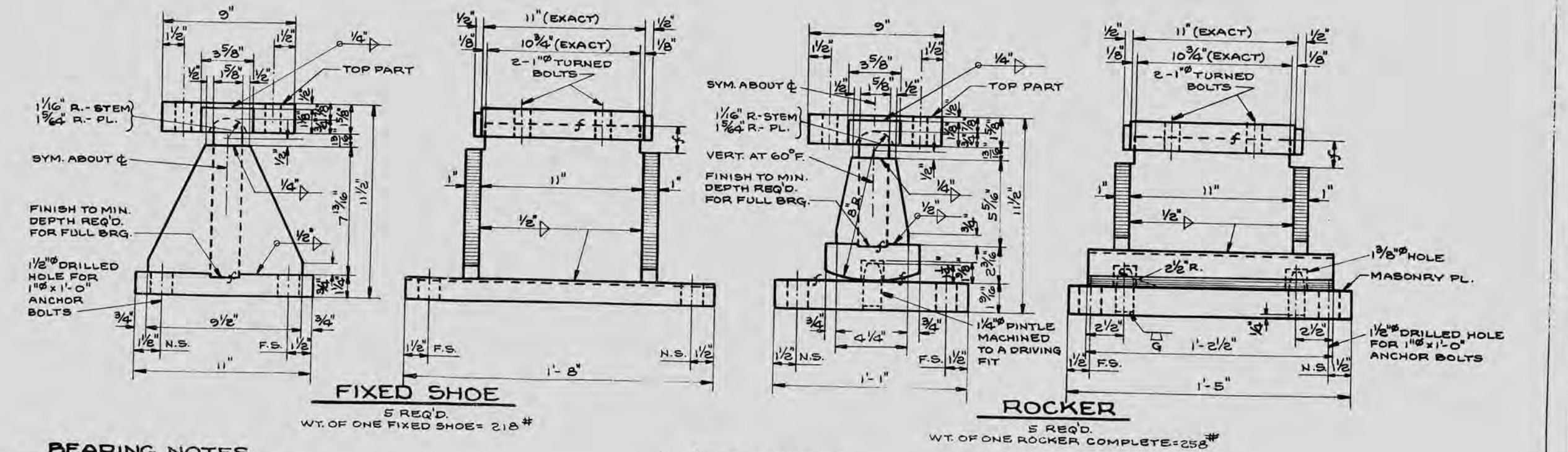
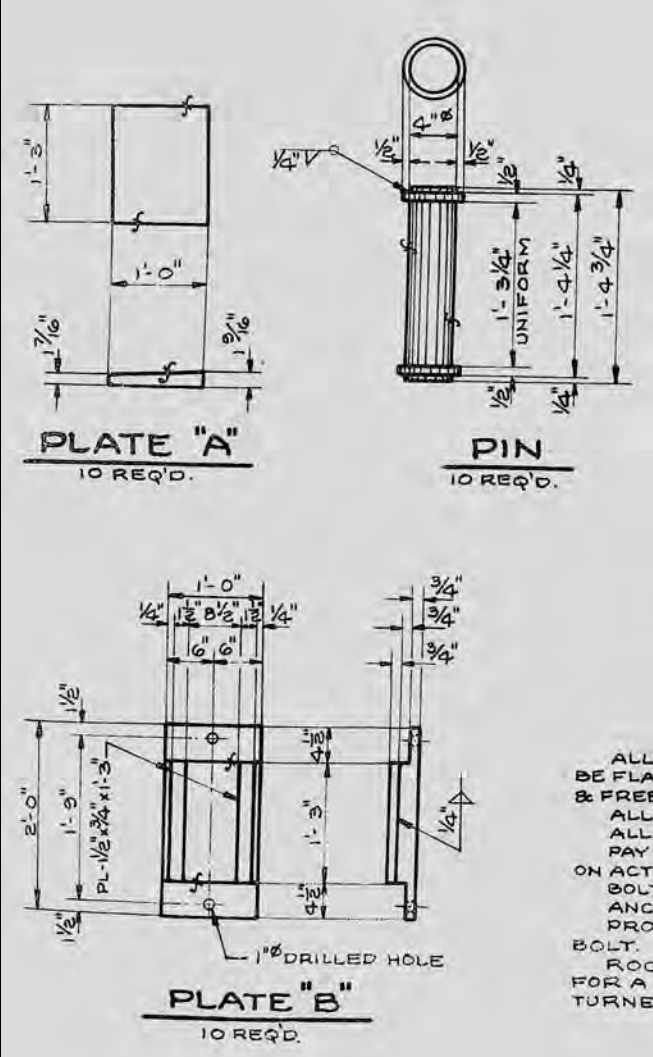
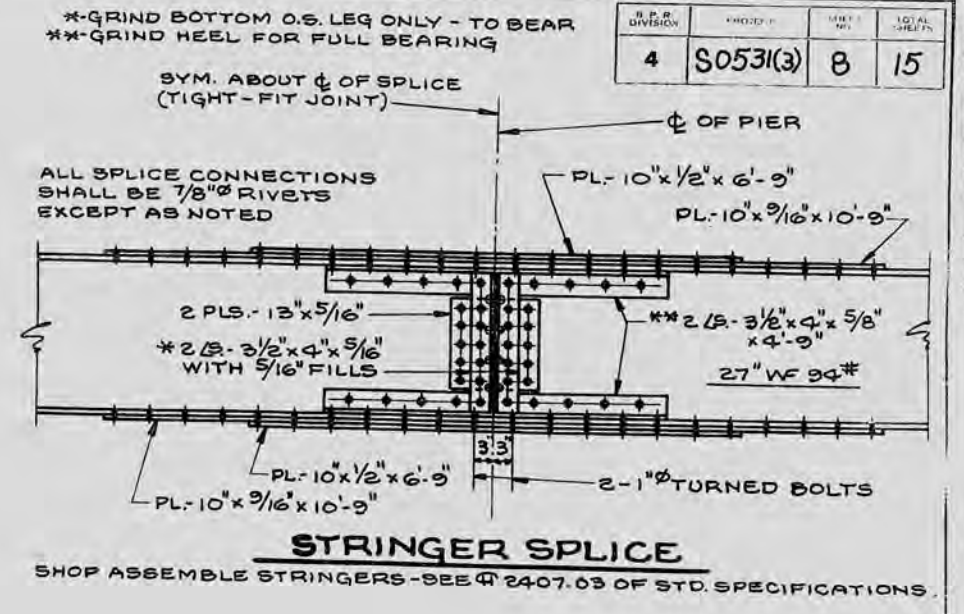
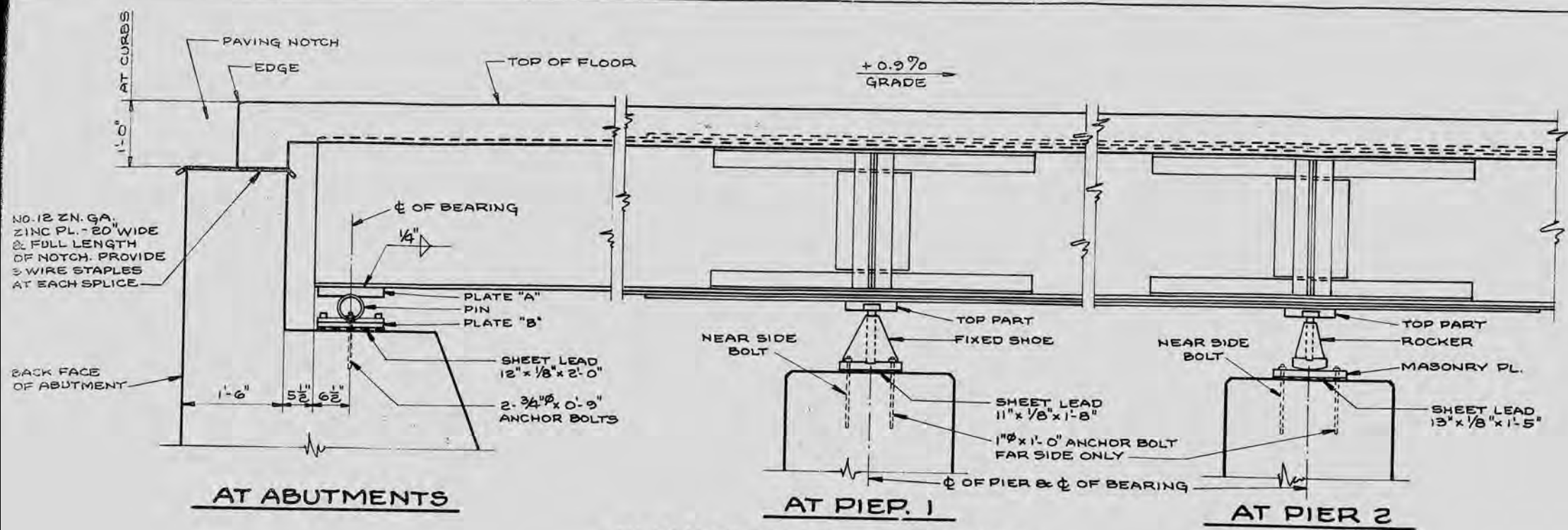
**SECTION S1**



**POURING DIAGRAM**

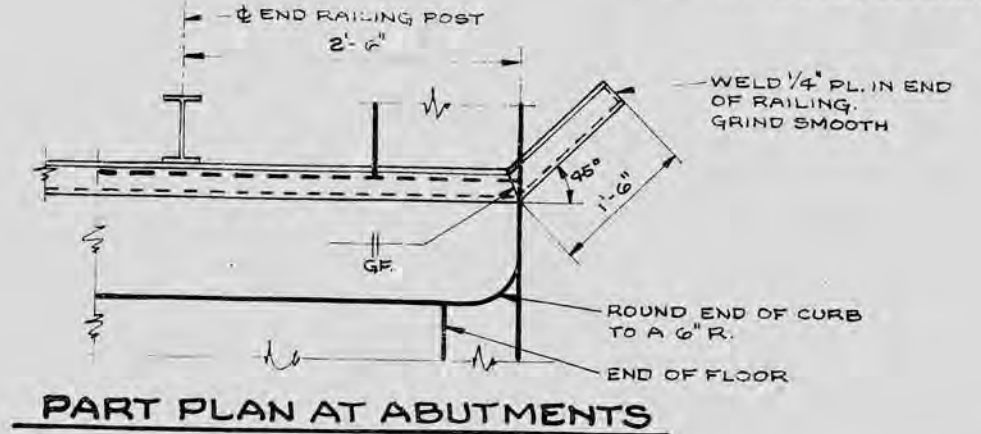
NOTE: FLOOR POURS SHALL BE MADE IN THE SEQUENCE SHOWN.

DESIGN DIVISION	PROJECT NO.	SHEET NO.	TOTAL SHEETS
4	S0531(3)	8	15



**BEARING NOTES**

ALL STRUCTURAL CARBON STEEL BEARING PLATES SHALL BE FLAT ROLLED STEEL PLATES WITH ALL SURFACES SMOOTH & FREE FROM WARP & ALL EDGES SMOOTH, STRAIGHT & VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE-FLAME CUTS. ALL SURFACES MARKED "f" SHALL BE MACHINE FINISHED. PAY QUANTITY FOR SIDE PLATES ON BEARINGS WILL BE BASED ON ACTUAL SIZE. BOLT TOP PLATE TO I-BEAM FOR SHIPMENT. ANCHOR BOLTS SHALL BE THREADED 3". PROVIDE ONE 1/4" WASHER & ONE HEX. NUT PER ANCHOR BOLT. ROCKER & FIXED SHOE WEIGHTS AS GIVEN ABOVE ARE FOR A COMPLETE BEARING INCLUDING ANCHOR BOLTS, TURNED BOLTS & PINTLES.



**BILL OF BARS**  
DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT

D.P.R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
4	S0531(3)	9	15

**ABUTMENTS**

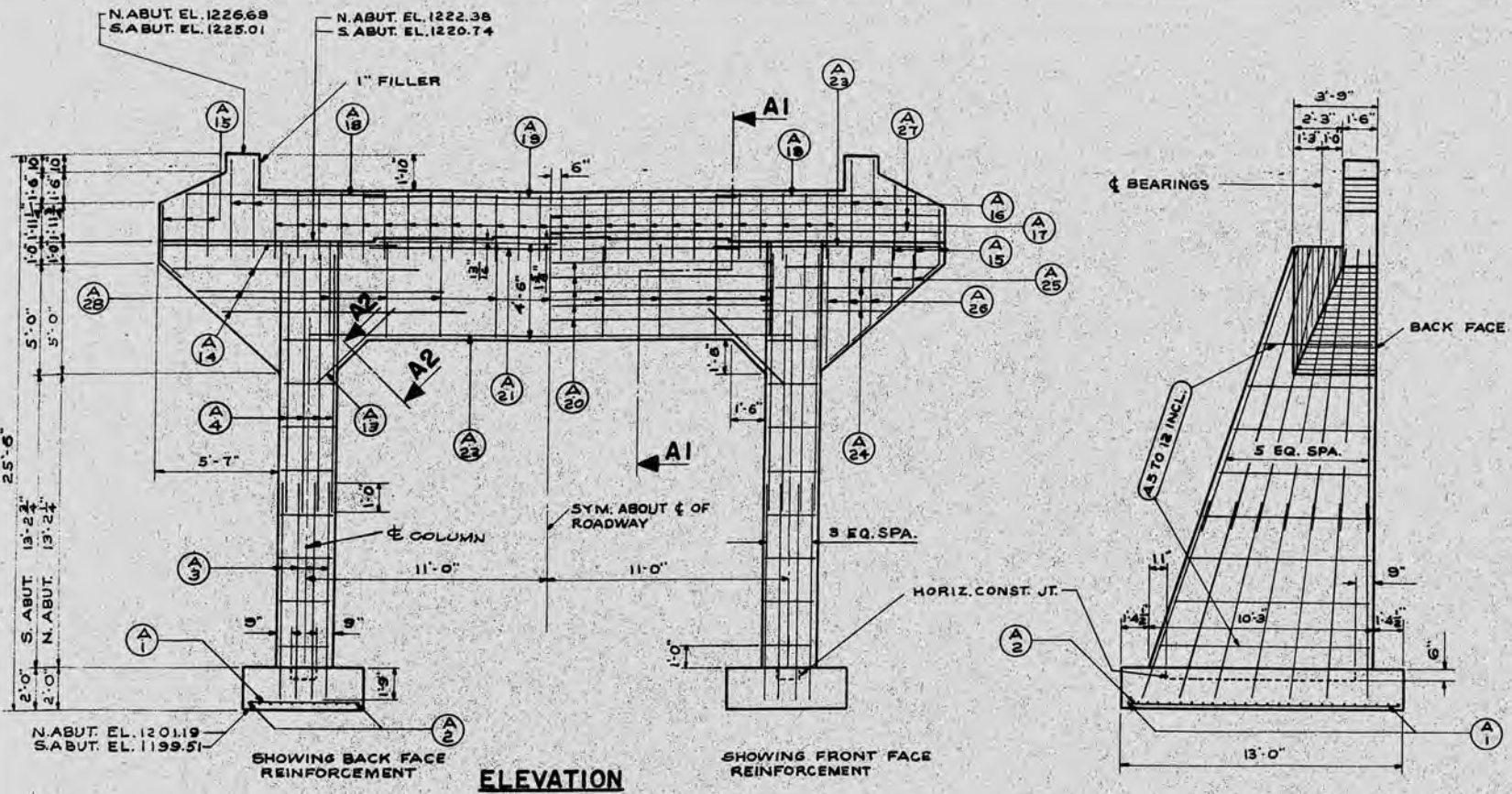
6,020#

**PIERS**

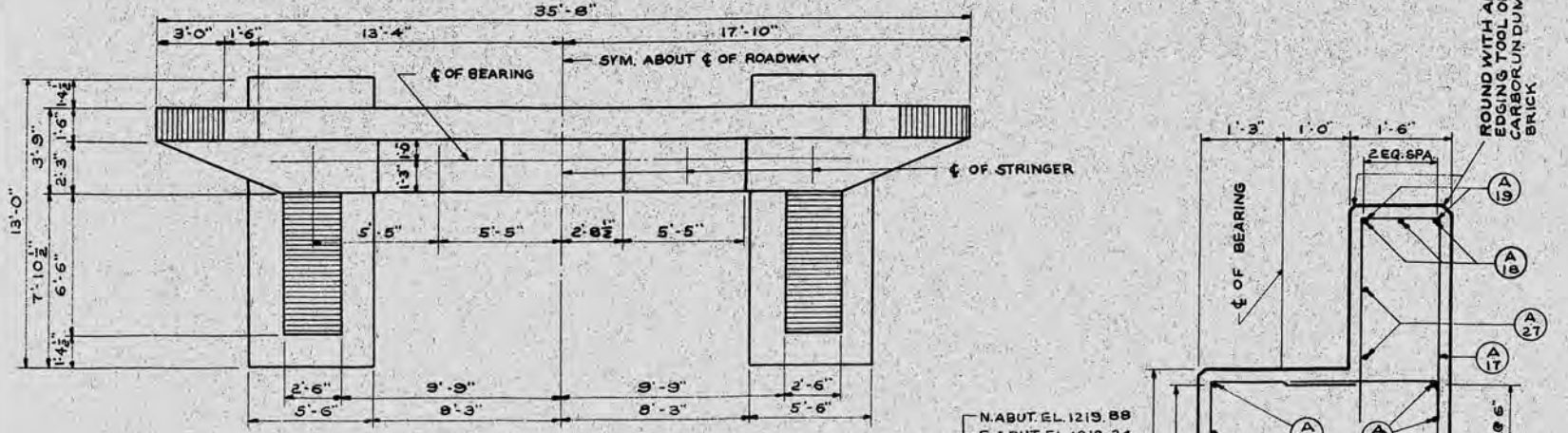
5,170#

POUR	MARK	NO.	SIZE NO.	LENGTH	SPACING	LOCATION	DET.
FOOTING	A 1	104	4	5-3	6	FOOTING	
	A 2	44	4	12-9	6	"	
	A 3	72	6	10-0	SHOWN	"	
	A 4	72	5	11-6	SHOWN	COLUMN	
	A 5	4	4	24-6	2-0	"	A
	A 6	4	4	23-3	2-0	"	A
	A 7	4	4	21-9	2-0	"	A
	A 8	4	4	20-6	2-0	"	A
	A 9	4	4	19-3	2-0	"	A
	A 10	4	4	17-9	2-0	"	A
	A 11	4	4	16-6	2-0	"	A
	A 12	4	4	15-3	2-0	"	A
	A 13	16	4	4-0	SHOWN	HAUNCH	
	A 14	16	6	10-6	6	WINGS B.F.	
	A 15	12	4	8-0	1-0	PARAPET	B
	A 16	8	4	11-6	1-0	"	B
	A 17	52	4	8-0	1-0	"	B
	A 18	12	6	9-0	SHOWN	" TOP	
	A 19	4	4	17-0	SHOWN	" TIE BAR	
	A 20	10	6	20-0	SHOWN	GIRDER F.F.	
	A 21	4	4	17-0	SHOWN	" & B.F. TIE BAR	
	A 22	16	8	22-0	SHOWN	" BOTTOM	
	A 23	4	4	10-0	1-0	" TOP F.F.	
	A 24	12	4	8-0	1-0	WING F.F.	
	A 25	8	4	3-0	1-0	"	
	A 26	12	4	5-0	1-0	"	
	A 27	8	4	17-9	1-0	PARAPET	
	A 28	18	4	16-3	2-6	GIRDER STIRRUPS	A
	A 29	60	4	5-0	SHOWN	GRID	C
	A 30	40	4	3-0	6	"	

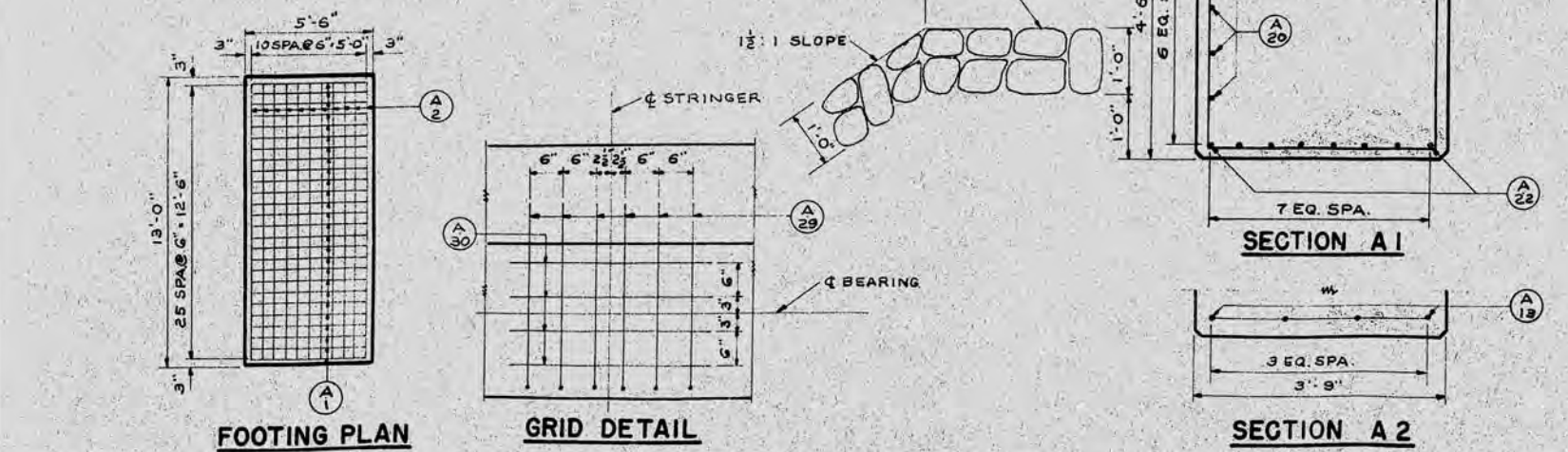
POUR	MARK	NO.	SIZE NO.	LENGTH	SPACING	LOCATION	DET.
FOOTING	P 1	44	5	6-3	9	FOOTING	
	P 2	36	5	8-0	9	"	
	P 3	56	5	10-0	SHOWN	" & COLUMN	
	P 4	28	7	12-0	SHOWN	COLUMN PIER #1 ONLY	
	P 5	28	7	12-6	SHOWN	" #2 "	
	P 6	72	4	8-3	2-0	"	F
	P 7	4	4	16-0	SHOWN	GIRDER	A
	P 8	4	4	15-0	SHOWN	"	A
	P 9	8	4	14-9	SHOWN	"	A
	P 10	10	10	22-6	SHOWN	"	H
	P 11	24	9	10-0	SHOWN	"	G
	P 12	4	4	13-6	SHOWN	"	
	P 13	36	4	2-6	SHOWN	GRID	
	P 14	50	4	4-9	SHOWN	"	E



**END VIEW**



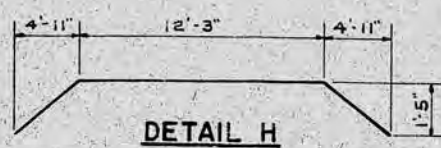
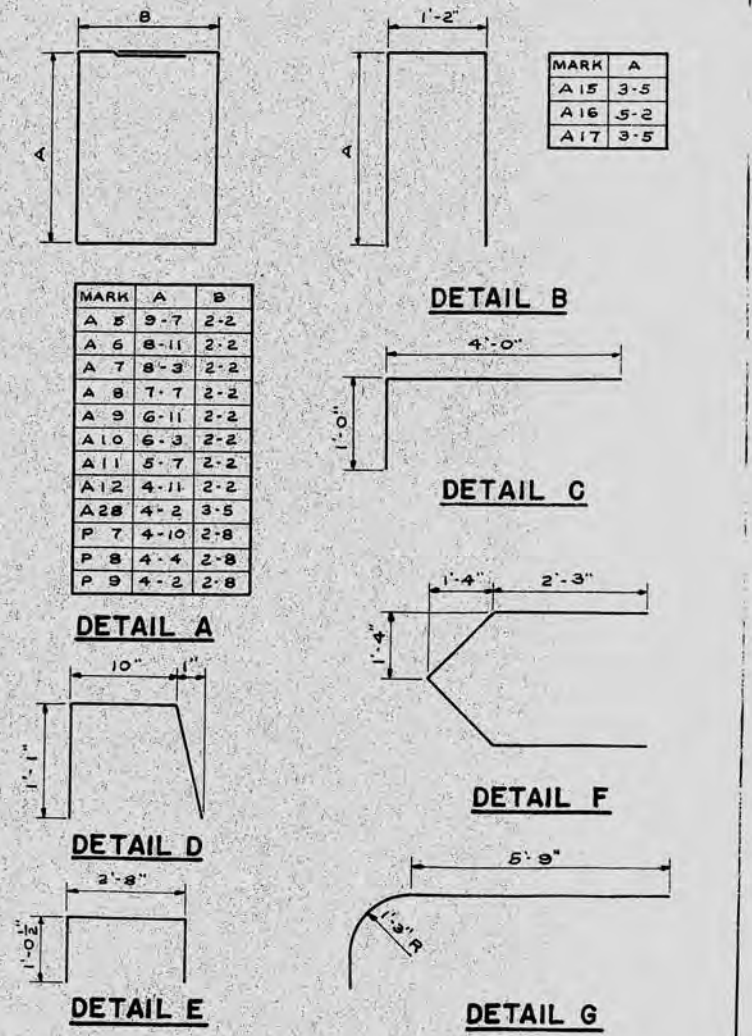
**PLAN**



**SUPERSTRUCTURE**

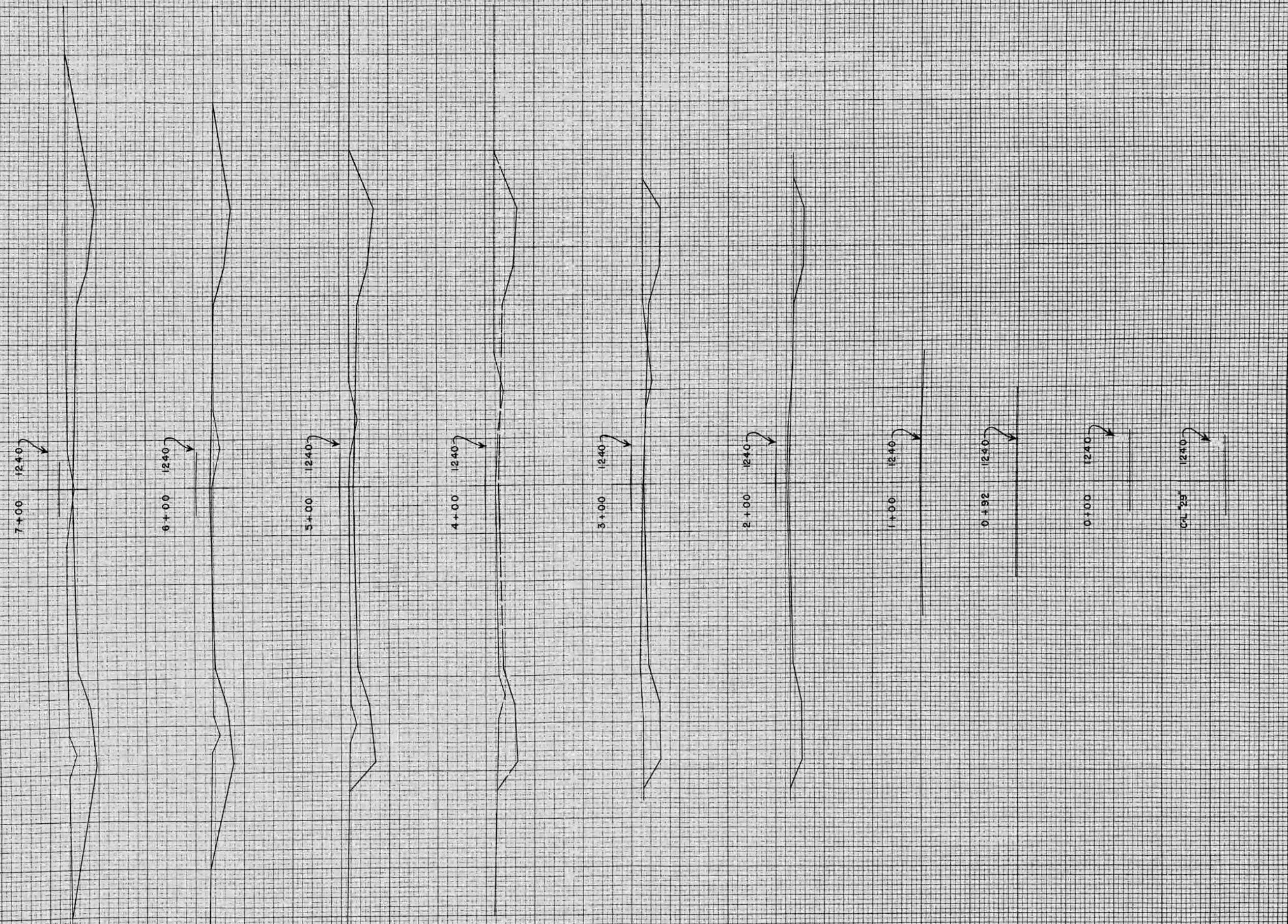
25,410#

POUR	MARK	NO.	SIZE NO.	LENGTH	SPACING	LOCATION	DET.
	S 1	106	5	26-0	7 1/2	FLOOR TOP & BOTTOM	
	S 2	34	5	32-0	1-6	"	
	S 4	40	4	32-0	7 1/2	" BOTTOM	
	S 5	46	4	3-0	1-6	CURB	D
	S 6	12	6	32-0	SHOWN	"	
	S 7	12	7	4-0	SHOWN	" SYM. ABOUT & POST	
	S 1	230	5	26-0	7 1/2	FLOOR TOP & BOTTOM	
	S 2	34	5	32-0	1-6	"	
	S 3	16	5	18-0	1-6	" SYM. ABOUT & PIER	
	S 4	40	4	32-0	7 1/2	" BOTTOM	
	S 5	96	4	3-0	1-6	CURB	D
	S 6	12	6	32-0	SHOWN	"	
	S 7	28	7	4-0	SHOWN	" SYM. ABOUT & POST	
	S 1	256	5	26-0	7 1/2	FLOOR TOP & BOTTOM	
	S 2	34	5	32-0	1-6	"	
	S 3	16	5	18-0	1-6	" SYM. ABOUT & PIER	
	S 4	40	4	32-0	7 1/2	" BOTTOM	
	S 5	108	4	3-0	1-6	CURB	D
	S 6	12	6	32-0	SHOWN	"	
	S 7	28	7	4-0	SHOWN	" SYM. ABOUT & POST	



**DETAIL H**

**X14500**



STATION	DISTANCE	YARDAGE		UNC.	FILL
		EXCAVATION	FILL		
0+00	7+00				
	600				
	500				
	400				
	300				
	200				
	100				
	0				
	630				
	530				
	430				
	330				
	230				
	130				
	30				
	0				
	25				
	15				
	5				
	0				
	630				
	530				
	430				
	330				
	230				
	130				
	30				
	0				
	25				
	15				
	5				
	0				
	630				
	530				
	430				
	330				
	230				
	130				
	30				
	0				
	25				
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	0				
	630				
	530				
	430				
	330				
	230				
	130				
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	630				
	530</				



16+00 1225

15+38 1225

15+30 1225

14+90 1225

14+52 1225

EL. TOP RIPRAP 1217.0

EL. TOP RIPRAP 1217.0

STATION	DISTANCE	YARDAGE		FILL
		EXCAVATION		
		UNC.		
15+12	22	0	0	188
14+90	38	0	0	533
14+52	32	0	0	286
15+00				

SHEET TOTAL

727

17+80 1225

17+70 1225

17+00 1225

16+78 1225

16+28 1225

EL. TOP RIPRAP 1217.0

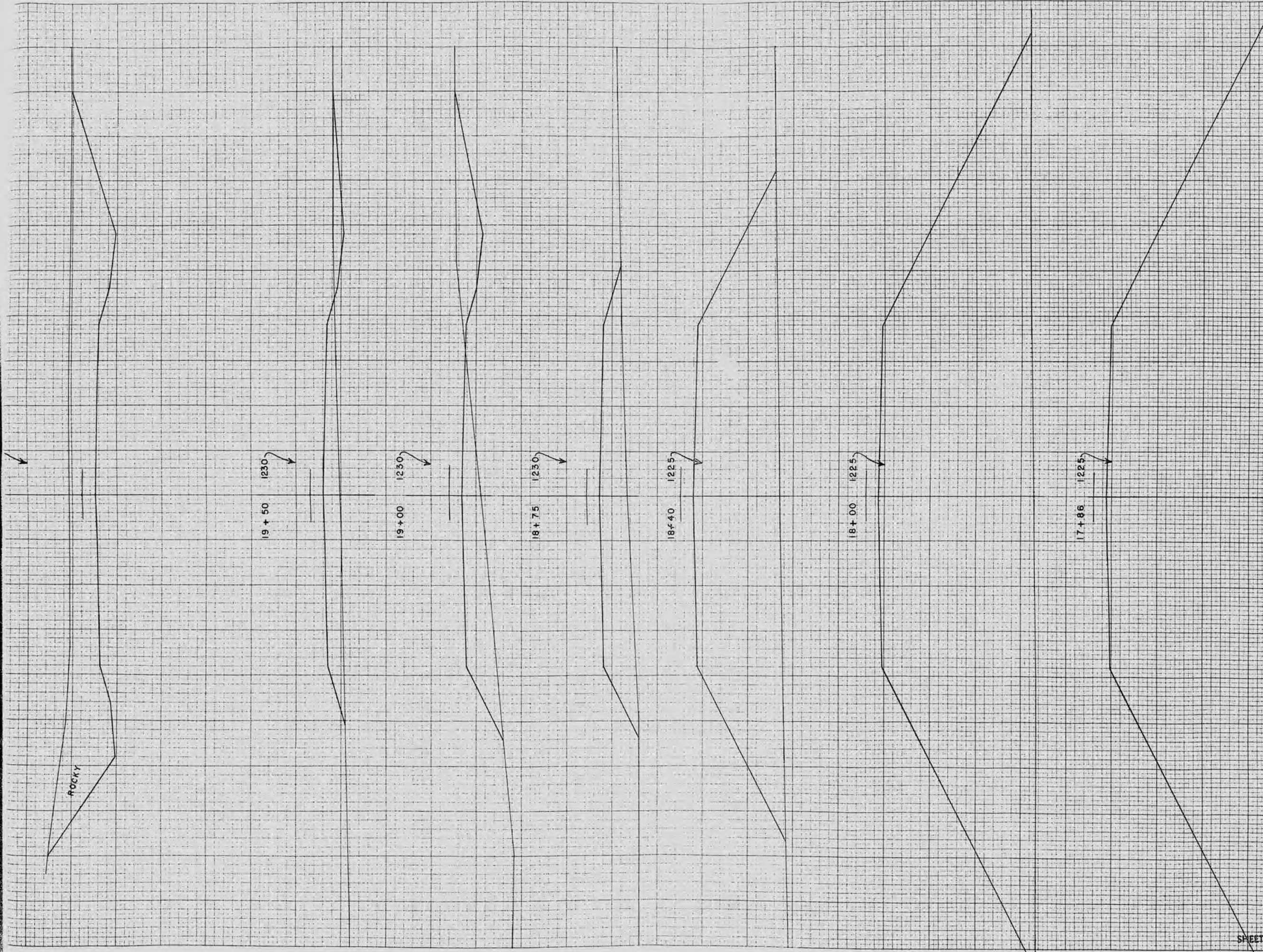
EL. TOP RIPRAP 1217.0

EL. TOP RIPRAP 1217.0

EL. TOP RIPRAP 1217.0

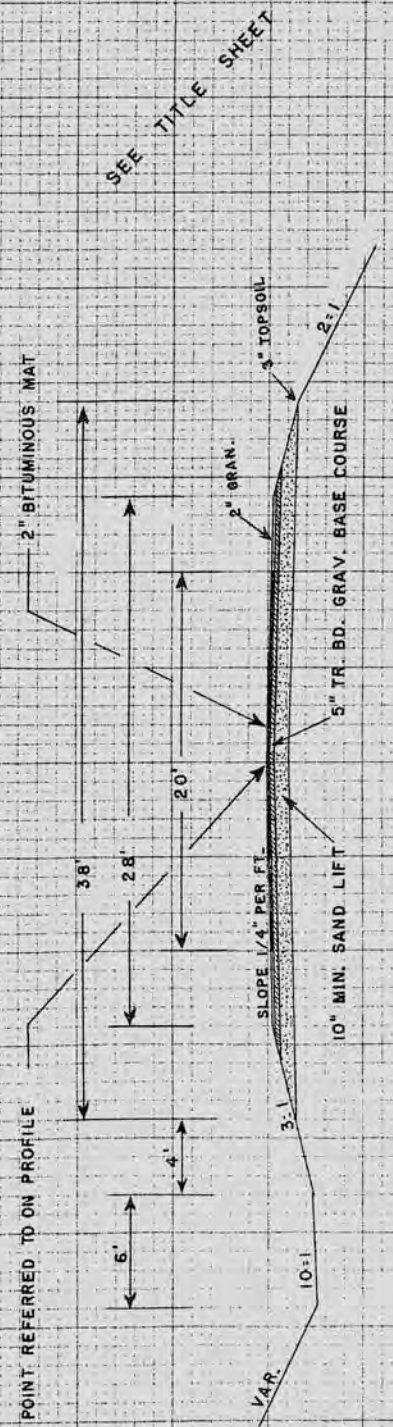
SHEET TOTAL 0 4400

STATION	DISTANCE	YARDAGE	
		EXCAVATION	
		UNC.	FILL
17+86			
17+80	6		
17+70	10		
17+00	70		
16+78	22		
16+55	23		
<b>TOTAL</b>		297	294
		364	586
		476	709
		2630	3287



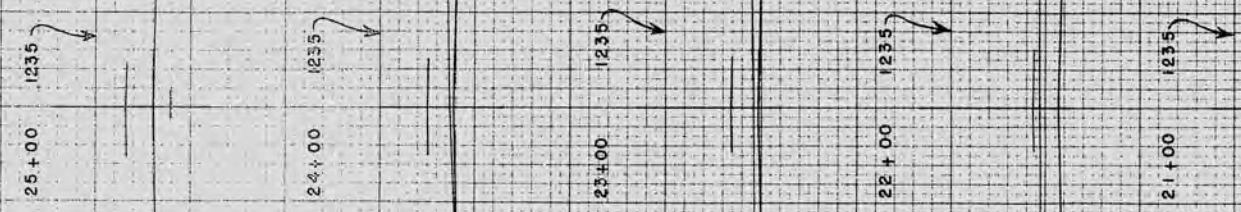
STATION	DISTANCE	YARDAGE	
		EXCAVATION	UNC.
21+00			
20+00	100		
19+50	50	56	
19+00	50		
18+75	25	20	
18+40	35	0	
18+00	40	0	
17+86	14	0	
17+80	6		
TOTAL	565	281	

SHEET TOTAL 1022



SEE TITLE SHEET

POINT REFERRED TO ON PROFILE



STATION	YARDAGE	
	EXCAVATION	UNC.
25+00		
24+00		
23+00		
22+00		
21+00		
TOTAL	407	0

SHEET TOTAL 407